



# LYONS NARROWS

Urban Design  
Guidelines

DRAFT – February 2022



**UPPER CANADA  
CONSULTANTS**  
ENGINEERS / PLANNERS

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## **1.0 OVERVIEW AND CONTEXT**

### **1.1 PURPOSE AND INTENT OF THE URBAN DESIGN GUIDELINES**

The Urban Design Guidelines (the “Guidelines”) pertain to the Lyons Narrows Development Area located between the Welland River and Lyons Creek in south Niagara Falls.

The intent and purpose of the Guidelines is to inform and direct public and private development in a manner that creates an aesthetically pleasing public realm, is compatible with existing development and the natural environment, and is pedestrian scaled.

The Guidelines have been prepared concurrently with Site Specific Zoning and are meant to provide guidance on built form, public spaces and road and streetscaping design.

Applications for Land Division, Building Permit and Site Plan Approval will be required to demonstrate compliance with the Guidelines.

The Guidelines are not meant to restrict specific Architectural or Urban Design approaches, but do prefer and seek a consistent palette of built form typology, overall.

## 1.2 APPLICABILITY

The Guidelines apply to the entirety of the Lyons Narrows Development Area (LNDA), which represents approximately 83 hectares of land in the southern end of Niagara Falls.

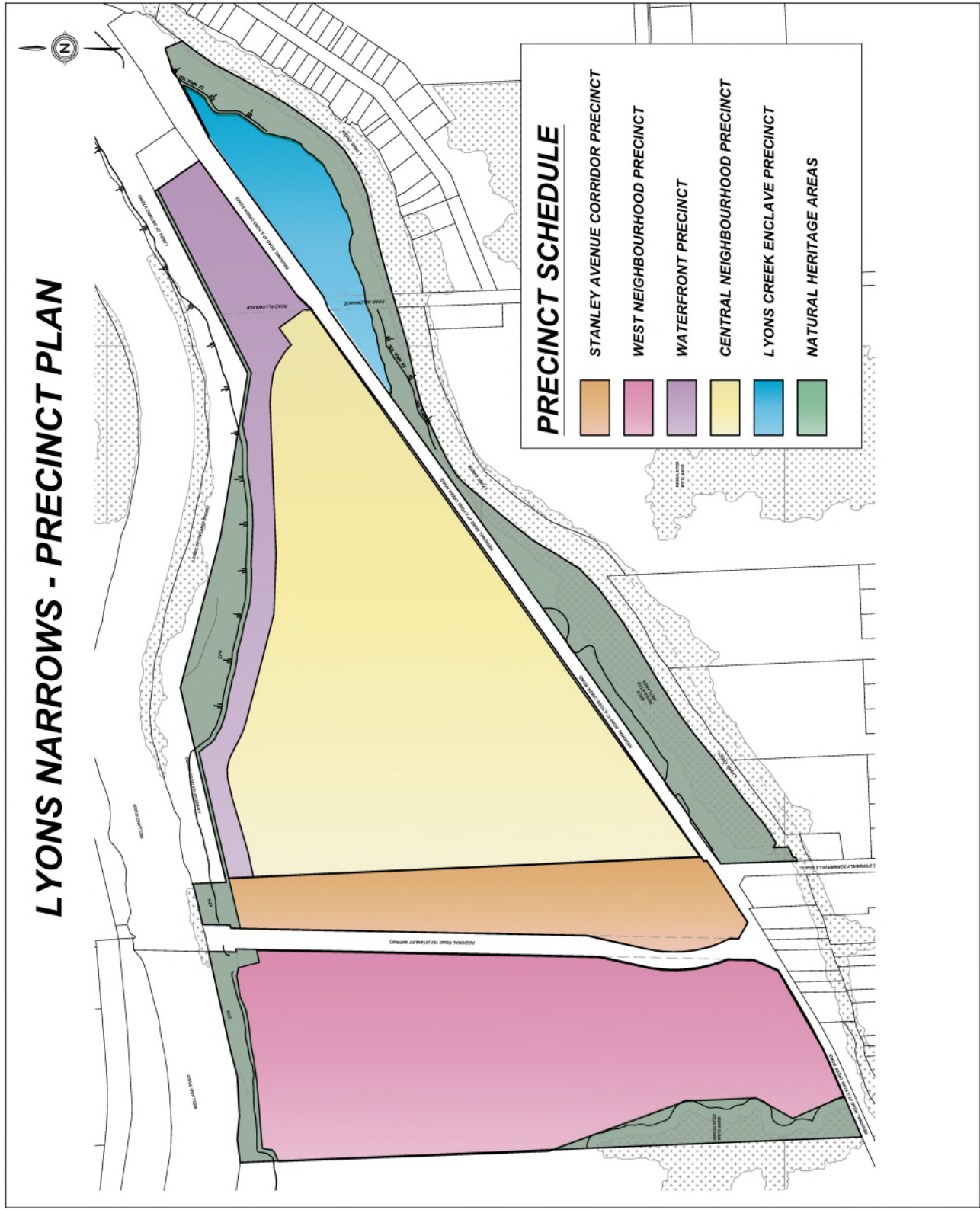
The LNDA area is located within the Niagara Falls Urban Area and Built-up area, thus technically qualifying all development as intensification. Due to the expansive and vacant nature of the development lands, which is not typically seen in the built-up area, a traditional greenfield/complete community approach to development is recommended for Lyons Narrows.

The LNDA has been divided into six (6) design **Precincts** in an effort to provide clarity and detail for certain land uses and physical conditions. These Precincts include:

- Stanley Avenue Corridor
- West Neighborhood
- Central Neighborhood
- Lyons Creek Enclave
- Waterfront
- Natural Heritage Areas

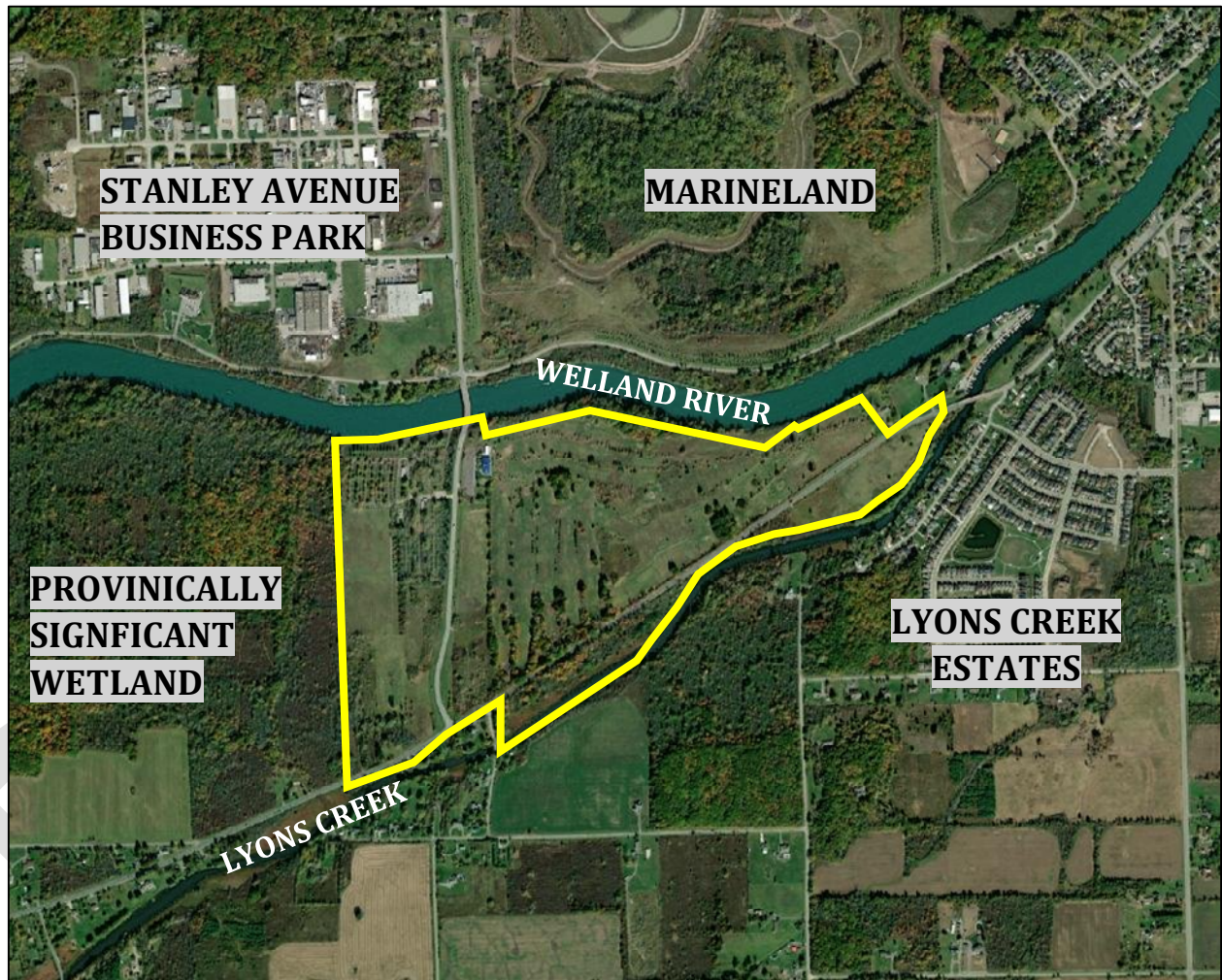
The boundaries of each design Precinct are based on physical features and general characteristics. These boundaries are not static and may be interpreted or refined by municipal staff or developers through the planning process. The Precinct Plan is shown on the following page.

# LYONS NARROWS - PRECINCT PLAN



### 1.3 DEVELOPMENT LIMITS AND AREA OF INFLUENCE

The limits of the LNDA are illustrated below in **Figure 1**.



*Figure 1 - Lyons Narrows Development Area (LNDA) Boundary*

#### 1.3.1 Surrounding Area

Lyons Narrows is situated in the southern end of Niagara Falls proximate to the confluence of the Welland River and Lyons Creek, west of the historic Village of Chippawa.



An aerial map of the surrounding area is shown as **Figure 2**.

Existing adjacent Land Uses include:

- North: Industrial (Stanley Avenue Business Park)
- West: Natural Heritage (Provincially Significant Wetland)
- South: Natural Heritage, Residential, Urban Boundary
- East: Residential (Lyons Creek Estates, Village of Chippawa)



*Figure 2 - Surrounding Area*

### 1.3.2 Transportation Network

The QEW highway and interchange is located approximately 2 kilometers west of the LNDA. This highway provides access to the northern and western parts of Niagara, and the Greater Toronto and Hamilton Area. The Highway also provides direct access to Fort Erie and the Canada-United States border.

The transportation network for this area, as shown on Schedule D of the City of Niagara Falls Official Plan is shown in **Figure 3**.



*Figure 3 - Schedule D of Niagara Falls Official Plan - Transportation Network*

The LNDA is intersected by Stanley Avenue (Regional Road 102) and Lyons Creek Road (Regional Road 47). Schedule D of the Niagara Falls Official Plan categorizes the portion of Stanley Avenue adjacent to the development lands as a Collector Road and Lyons Creek Road as an Arterial Roadway. All future roads supporting the LNDA will be classified as local roads.

## **2.0 PUBLIC REALM AND COMMUNITY ACCESS**

The Public Realm is defined within these Guidelines as the area that is both physically and visually accessible to individuals within the LNDA. In the interest of creating vibrant streetscapes, inviting public spaces and safe routes of travel, an emphasis should be placed on placemaking.

### **2.1 ROADWAYS**

Consistent with the Niagara Falls Official Plan, the following Road Classifications exist within the municipality:

- Highway
- Arterial Road
- Collector Road
- Local Road

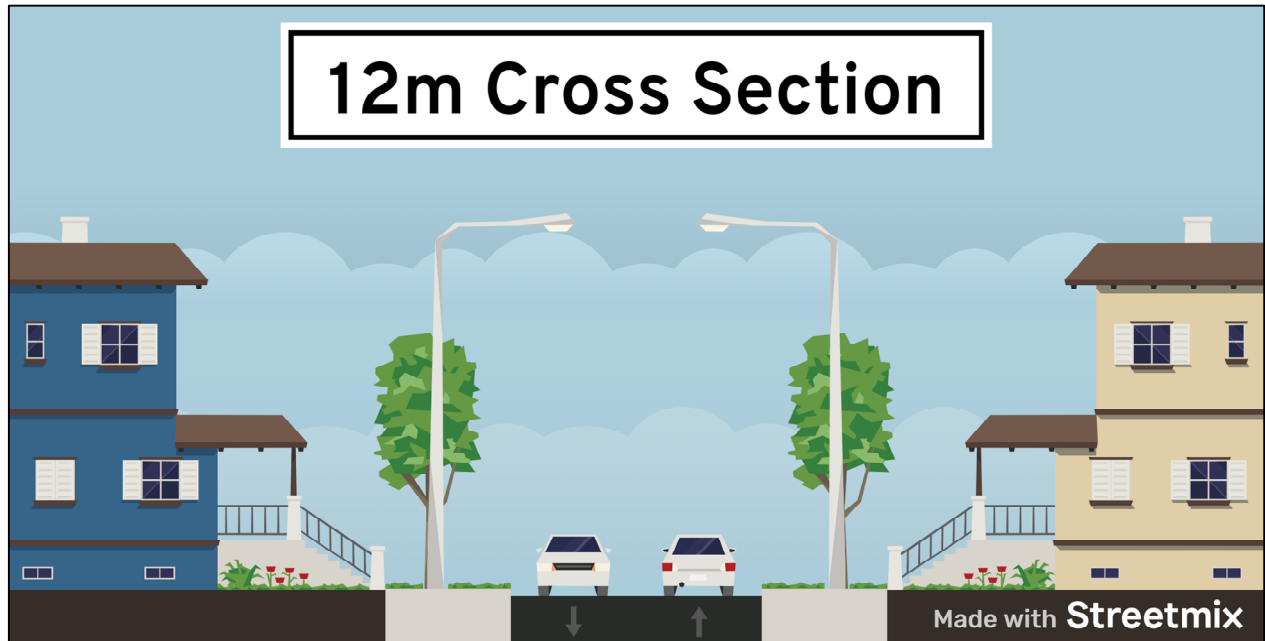
The cross sections for Stanley Avenue and Lyons Creek Road are set and designed by Niagara Region.

Within the LNDA, new local roads will be constructed to provide access throughout the neighbourhood. Within the neighbourhood, three (3) road cross sections are recommended to support the complete streets and accessibility goals for the public realm.

These cross sections include 12, 18 and 22 metre street designs. Illustrative graphics of each street type are provided within this section of the Guidelines. Detailed typical engineering cross sections utilized by the City of Niagara Falls are included within **Appendix X** for reference.

## **12.0 metre Cross Section - Private Road**

The 12.0 metre cross section is the typical private roadway that would be found within condominium developments.

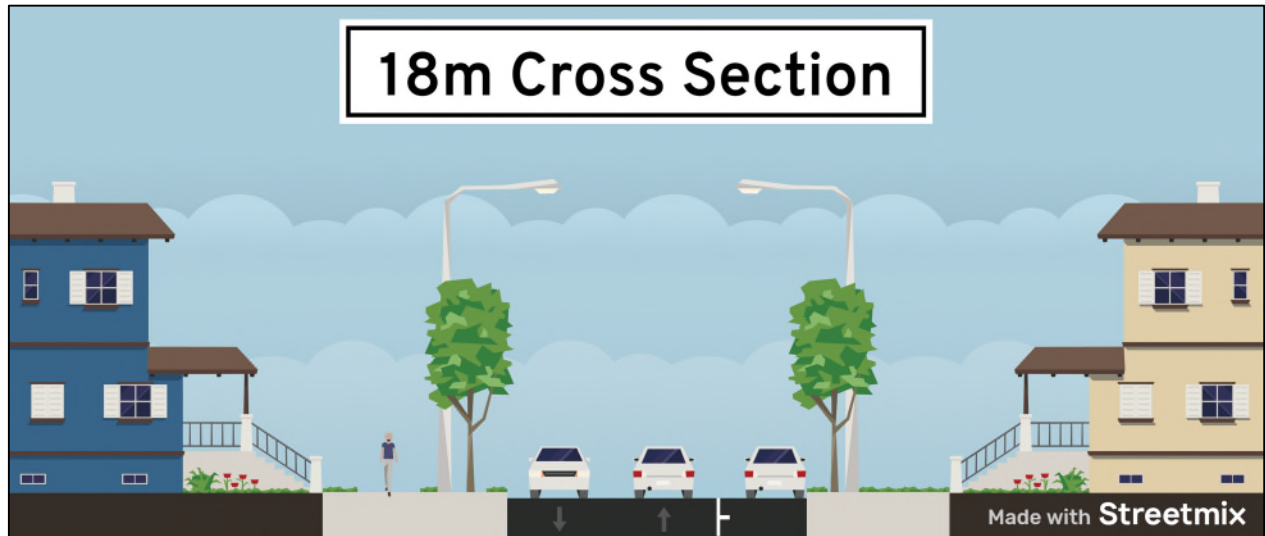


Street elements should include:

- Two Drive Lanes
- Street Trees on both sides of the roadway
- Street Lights on both sides of the roadway

## **18.0 metre Cross Section – Local Roads**

The 18.0 metre cross section is the typical local, residential street that would be found within the LNDA.

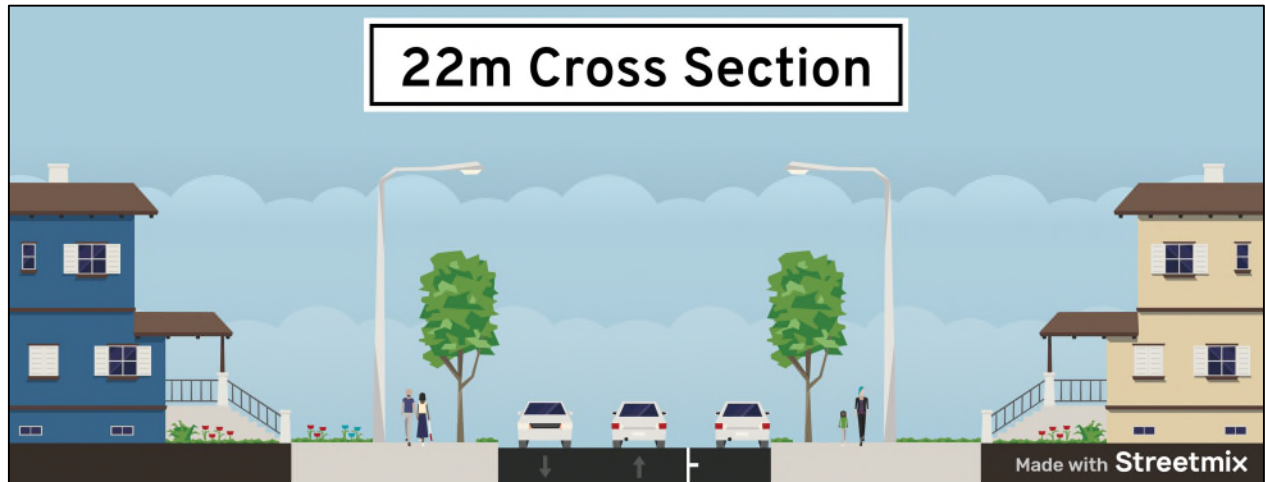


Street elements should include:

- Two Drive Lanes
- One Lane of on-street parking
- Street Trees on both sides of the roadway
- Street Lights on both sides of the roadway
- A minimum 1.8 metre sidewalk on one side of the street
- An suitably sized boulevard for utilities (transformers, lighting, fire hydrant, telecom) and street furniture (benches, waste receptacles).

## **22.0 metre Cross Section - Primary Roads**

The 22.0 metre cross section is the typical priority or main residential street that would be found within the LNDA.



Street elements should include:

- Two Drive Lanes
- One Lane of on-street parking
- Street Trees on both sides of the roadway
- Street Lights on both sides of the roadway
- A minimum 1.8 metre sidewalk on both side of the street
- An suitably sized boulevard for utilities (transformers, lighting, fire hydrant, telecom) and street furniture (benches, waste receptacles).

## 2.2 STREETSCAPING

Along existing and planned roads, developers, the City and Region are encouraged to coordinate public realm investment that creates inviting, accessible and well designed public spaces.

The following Guidelines should be considered with development applications and road projects.

### 2.2.1 Building Orientation

- All buildings should be oriented with their functional or architectural front facing the public realm.
- Corner lots should focus design excellence and pedestrian access from the longest frontage to provide the most street framing.
- Direct Pedestrian connections to all buildings from municipal sidewalks are encouraged.
- Glazing should be directed towards the public realm. “Fake” windows are not acceptable along priority roads.

### 2.2.2 Public Transit

- Public Transit Shelters shall be provided along transit routes.
- Transit Shelters shall be located next to street lighting, or have their own lighting provided.

### 2.2.3 Active Transportation

- Sidewalks of a minimum width of 1.8 metres shall be provided on both sides of primary roads (22.0 metre cross section).
- Bicycle racks should be provided at transit stops and parks.

### 2.2.4 Street Furniture

- The provision of benches and waste receptacles is encouraged along local roadways. Priority areas for the provision of these elements should be near parkland, key vistas, or every 100 metres along streets.
- Pedestrian oriented street lighting is encouraged near areas containing street furniture or transit shelters.
- Community mailboxes are encouraged to be located near benches and waste receptacles.

### 2.2.5 Trees and Plantings

- Boulevard plantings shall be provided throughout Lyons narrows to enhance the urban tree canopy.
- Each lot should be provided with a street tree, or landscape feature.
- Native species shall be the only permitted form of trees within the public realm.



- Deciduous trees are preferred along streets. Coniferous trees may be permitted, provided they do not obstruct views, traffic sight lines or create unsafe environments (CPTED)

#### 2.2.6 Public Art

- Public Art is encouraged to be provided within the development areas at key intersections or places of high volumes of pedestrian traffic.
- Utility Boxes shall be permitted to be painted or wrapped with art. No commercial advertisements are permitted.

## 2.3 GATEWAYS

### 2.3.1 Regional Road Gateways

Lyons Narrows will be primarily accessed from Stanley Avenue and Lyons Creek Road, which are Regional roadways. Where proposed public streets or private development driveways intersect with these roadways, enhanced urban design and streetscaping elements are encouraged, including:

- The provision of low walls or neighbourhood identification at Regional Gateways.
- Planted medians, inclusive of street lighting are encouraged at Regional Gateways
- Enhanced architectural elements and treatments to establish design excellence adjacent to Regional Gateways.
- Large scale roundabouts, inclusive of plantings or public art.

### 2.3.2 Local Road Gateways

Where new local streets converge or intersect, additional design consideration should be pursued, including:

- Small scale roundabouts.
- Planted medians or boulevards.
- Banners or public art.

### 2.3.3 Trail Gateways

- Access points to trails should be clearly identified with signage and lighting.
- Bollards or similar barriers should be provided where trails intersect with roadways, to limit unauthorized use of trails and to signify traffic crossing.
- Street furniture such as waste receptacles, benches and bicycle racks should be provided at trail accesses.

## 2.4 STORMWATER MANAGEMENT FACILITIES

Stormwater Management Facilities are integral to the safe and sustainable development of neighborhoods.

Stormwater Management Facilities function in a manner that improves the quality of stormwater runoff and can also manage quantiles before being discharged to an appropriate outlet.

From an aesthetic perspective, Stormwater Management facilities such as ponds and channels provide a passive opportunity for enhanced landscaping and ecological benefit. These facilities can also serve as visually pleasing, natural backdrops and vistas for adjacent developments

### 2.4.1 Design Principles

To efficiently utilize required Stormwater Management Facilities in a manner that contributes to efficient and aesthetically pleasing development, development proposals should:

- Integrate Stormwater Management Facilities as part of the neighbourhood as significant landscape features or Open Space.
- Secure facilities with fencing or barriers that are visually appealing and visually permeable (i.e. black vinyl chain-link fencing rather than wood board fencing) where security is necessary. Where not deemed necessary, Stormwater Management Facilities should not be fenced.

- Integrate stormwater ponds as part of proposed trails and walking routes, provided that safety measures are in place.

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## 2.5 PARKS AND OPEN SPACE

Parks and Open Space contribute to the aesthetic appeal and overall enjoyment of neighbourhoods by residents.

### 2.5.1 Design Principles

The inclusion of Parks of varying scales, Trails and other Open Space areas are encouraged within the development and are subject to the following Guidelines:

- Parkland, Open Spaces and Trails should be located within no further than 300 metres of residential development.
- Where development lots are adjacent to parks, fencing shall be provided to prevent trespass. Visual permeable fencing is encouraged to improve passive visual surveillance of the park.
- A large-scale municipal park (greater than 1 hectare) shall be provided within the LNDA.
- Parks and Open Spaces should be connected to and accessible from active transportation networks including trails, sidewalks or bicycle routes.
- Parks should provide through connections for pedestrians.
- Parkland areas should include recreational equipment and facilities for persons of all ages.

- The inclusion of the planting of native tree species is strongly encouraged within Parks and along trails to contribute to the urban tree canopy.
- Parks shall include benches, waste receptacles, bicycle racks and other typical amenities.
- Large Neighbourhood Parks shall have public road frontage on at least two sides.

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### **3.0 URBAN DESIGN PRINCIPLES AND PRIORITIES**

The Guidelines are based on five design and development (5) principles, which include:

1. A mix and range of housing throughout neighbourhood blocks.
2. An open, inviting and pedestrian scaled public realm.
3. Ensuring connectivity and linkages between Precincts, open spaces and the natural environment.
4. Providing quality views and vistas; *and*
5. Implementing Complete Streets.

#### **3.0.1 Priorities for Residential Development**

Although the Zoning for Lyons Narrows is permissive of most residential dwelling types, each set of Precinct Guidelines identifies the priority, or preferred forms of residential dwellings for that specific area.

These Guidelines are not intended to prohibit permitted residential land uses not cited as “priority”, however, developers should provide clear justification for the inclusion of non-priority dwelling types through their application submissions.

#### **3.0.2 Priority Properties**

Priority properties are those lots or blocks that have the highest visual exposure from the public realm (i.e. corner lots, terminating vistas). These lots should be developed in a manner that includes high quality architectural design and



materials and landscaping. The location of these priority properties should be provided on development plans (i.e. subdivision plan, block plan, site plan).

Priority Properties should provide high quality design on all sides of the dwelling or building which faces a public street or park. Rear walls are exempt from this requirement.

### 3.0.3 Priority Roads

Priority Roads shall include the “main” or longest continual roadways within the LNDA, or roadways which intersect with Stanley Avenue and Lyons Creek Road. These roads are the veins of the development and shall include street furniture, boulevard plantings, wayfinding and traffic calming measures, as appropriate.

### 3.0.4 Priorities for the Natural Environment

The natural backdrop for the LNDA is of immense value to the municipality and residents of Niagara Falls. Through development and design, steps shall be taken to implement required natural heritage protection measures prescribed through professional studies. Opportunities for improving the quality of, and access to natural heritage areas within the LNDA are strongly encouraged.

### 3.1 PERMITTED LAND USES

As set forth in the Site-specific Zoning By-law provisions, the following land uses are permitted within the LNDA:

#### Residential

- Single Detached Dwelling;
- Semi-Detached Dwellings;
- On-Street Townhouses;
- Block Townhouses; *and*
- Apartments.

#### Commercial (per Neighbourhood Commercial “NC” Zone)

- Bake Shop
- Bank
- Clinic
- Library
- Office
- Personal Service shop
- Restaurant
- Retail store
- Service shop
- Animal clinic
- Dwelling units
- Day nursery
- Health Centre

### Institutional (per Institutional “I” Zone)

- Places of Worship
- Schools
- Day Care
- Assisted Living
- Long Term Care Homes

### Parks and Open Spaces

- Neighbourhood Parks
- Trails

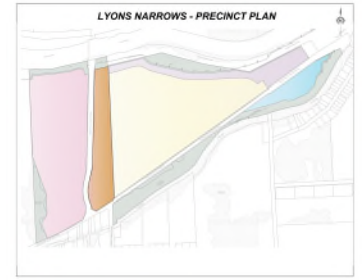
### Natural Heritage System

- Features and their buffers as identified through Environmental Impact Studies.

### Ancillary Uses and Features

- Linear Utilities
- Stormwater Management Facilities

## 3.2 STANLEY AVENUE CORRIDOR PRECINCT



The Stanley Avenue Precinct is located along the east side of Stanley Avenue between the roadway and the Enbridge Utility Corridor. This area is best situated to accommodate transit-supportive development residential and employment densities and contain a mix of residential, mixed-use, institutional and commercial land uses.

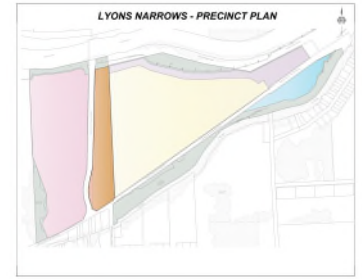
### 3.2.1 Medium to High Rise Development

The following land uses should be developed within the Stanley Avenue Corridor Precinct.

- Apartment Dwellings;
- On-street Townhouses;
- Block Townhouses;
- Commercial Uses;
- Institutional Uses; *and*
- Parks and Open Space.

### 3.2.2 Design Principles

- The Stanley Avenue Corridor Precinct should include Apartment dwellings and mixed-use buildings between 3 to 5 storeys in height.
- Significant shadowing and overlook impacts on adjacent development and roadways should be avoided where possible. However, it is anticipated that sine off-site shadow impacts will occur.



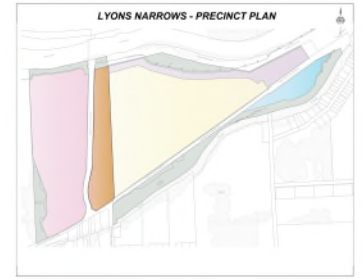
- Buildings should include significant glazing, balconies and pedestrian accesses where possible along Stanley Avenue.
- Ground level commercial uses are encouraged along Stanley Avenue within mixed-use/apartment buildings.

### 3.2.3 Accessibility and Connectivity

- Dwellings and buildings should provide dedicated pedestrian connections to sidewalks, trails or bicycle and vehicle parking areas. These areas should be designed in a manner that provides seasonal protection from wind, snow and rain, where appropriate, and contain pedestrian oriented lighting.
- Where Public Transit service is available, opportunities for direct pedestrian connections and transit supportive street furniture are strongly encouraged to be included in development plans or road improvement projects (i.e. benches, waste receptible, transit shelters).
- Driveway accesses to lots, blocks and buildings should not be provided directly from Stanley Avenue or Lyons Creek Road. Driveways or shared accesses from local roadways are preferred.

### 3.2.4 Parking

- Surface parking should be provided out of view from Stanley Avenue, preferably behind Buildings.



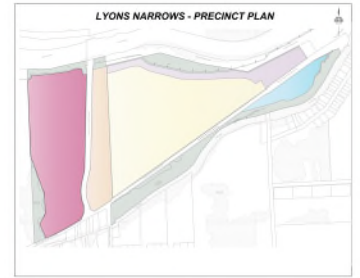
- Surface parking is permitted along the sides of apartment or commercial buildings, but should be screened with landscaping or other materials that positively contribute to the public realm.
- Accessible and Green Vehicle parking areas should be provided with priority locations on sites, but should be appropriately screened from any public views.
- Parking lots should include landscaping and dedicated pedestrian access routes.
- Limited visitor / commercial parking spaces are permitted between the building and Stanley Avenue.

### 3.2.5 Loading Areas

- Required loading areas and spaces should be located out of view from the public realm.
- Uses requiring dedicated loading facilities (i.e. loading dock) shall ensure appropriate visual and noise mitigation measures are proposed to ensure compatibility with sensitive land uses.

### 3.3 WEST NEIGHBOURHOOD PRECINCT

The West Neighbourhood area is located on the west side of Stanley Avenue between Lyons Creek Road and the Welland River.



This portion of the subject lands is envisioned to contain primarily residential land uses, including parkland, and a local street network.

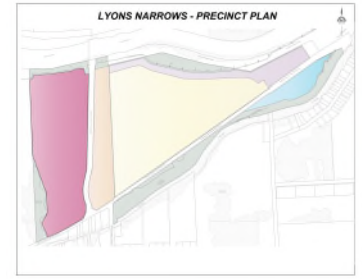
#### 3.3.1 Low Rise Development

The following land uses should be developed within the West Neighbourhood Precinct:

- Single Detached Dwellings;
- Semi-Detached Dwellings;
- On-Street Townhouses;
- Small Scale Apartments;
- Open Spaces (Parkland, Trails)

#### 3.3.2 Design Principles

- Reverse lotting shall not be permitted along Stanley Avenue.
- Window Streets are permitted within the West neighbourhood, but should not extend along more than one development block (i.e. alternating windows streets are preferred).
- Where garages are provided in the front yard, garages shall be located a minimum of 1.5 back from the front wall of a dwelling or porch.

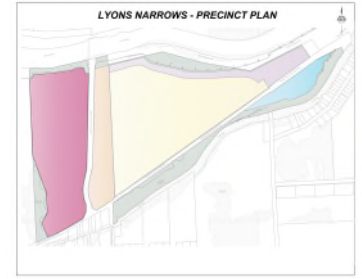


- Where semi-detached or townhouse dwellings are proposed, the twinning of driveways is encouraged.
- Townhouse blocks should not exceed 4 units in a row.
- Small Scale Apartment dwellings (3 storeys or less) are encouraged to be located close to public roadways and be sited in a manner that limits overlook on adjacent development.
- Architectural designs should vary throughout a street block, and should not repeat more than two (2) times in a row.
- Variations in roof lines, or the inclusion of upper level design elements such as transom windows and dormers is encouraged to provide variation in building height and form.
- Consistent fencing types and materials are strongly encouraged within developments to ensure a uniform look and feel. Impermeable/solid fencing such as wood or masonry walls should not be used to screen private amenity yards from flanking streets.

### 3.3.3 Accessibility and Connectivity

- Dwellings and buildings should provide dedicated pedestrian connections to sidewalks.
- Higher density land uses such as townhouses or apartments should be located in close proximity to public transit stops.



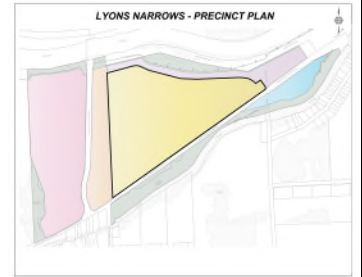


### 3.3.4 Parking

- Low Density Residential uses such as Single Detached Dwellings, Semi-Detached Dwellings and On-Street Townhomes should provide attached garages.
- Detached garages should only be permitted in rear lane configurations.
- Driveways should not exceed 65% of the width of a lot.

### 3.3.5 Rear Lane Dwellings

- Rear lane access and garages are acceptable within the West Neighbourhood, but should be provided on a full block basis.
- Where rear lane developments are proposed, dwellings should be located at the minimum front yard setback prescribed in the Zoning By-law.
- Dwellings with rear lane access should provide higher quality architectural design including traditional building materials or looks such as brick, stone and wood.
- Front Porches and/or balconies are should be provided for rear lane dwellings.



### 3.4 CENTRAL NEIGHBOURHOOD PRECINCT

The Central Neighbourhood area is generally located east of the Enbridge Utility corridor, north of Lyons Creek Road and extends to the municipal road allowance. This portion of the subject lands is envisioned to contain primarily residential development, including parkland, stormwater management facilities and a local street networks.

#### 3.4.1 Low Rise Development

The following land uses should be developed within the Central Neighbourhood Precinct:

- Single Detached Dwellings;
- Semi-Detached Dwellings;
- On-Street Townhouses;
- Small Scale Apartments; *and*
- Open Spaces (Parkland, Trails).

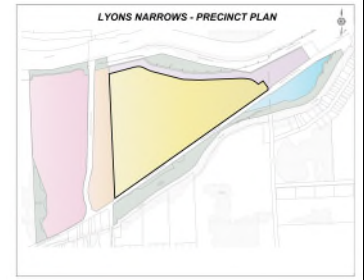
#### 3.4.2 Design Principles

- Where garages are provided in the front yard, they should be located a minimum of 1.5 back from the front wall or porch of the dwelling.
- Where townhouse dwellings are proposed, the twinning of driveways is encouraged.
- Townhouse blocks should not exceed 4 units in a row.

#### 3.4.3 Accessibility and Connectivity

- Dwellings and buildings should provide dedicated pedestrian connections to sidewalks, trails or bicycle

and vehicle parking areas. These areas should be designed in a manner that provides seasonal protection from wind and rain, where appropriate, and contain lighting, where appropriate.



- Where Public Transit service is available or proposed, opportunities for direct pedestrian connections and transit supportive street furniture are strongly encouraged to be included in development plans or road improvement projects (i.e. benches, waste receptible, transit shelters)

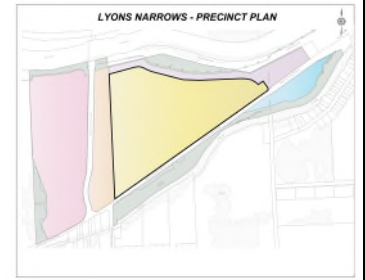
#### 3.4.4 Parking

- Low Density Residential uses such as Single Detached Dwellings, Semi-Detached Dwellings and On-Street Townhomes should provide parking within private garages.
- On-street parking should be provided on one side of the street within the Central neighbourhood.

#### 3.4.5 Rear Lane Dwellings

- Rear lane accesses and garages are acceptable within the LNDA and should be provided on a full block basis.
- Where rear lane developments are proposed, dwellings should be located at the minimum front yard setback prescribed in the Zoning By-law to create prominence in the built form along the street.

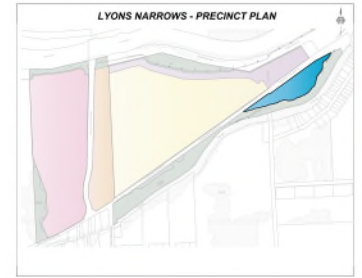
- Dwellings with rear lane access or garages should provide higher quality architectural design on the front façade, including traditional building materials such as brick, stone and wood. Front porches and balconies are strongly encouraged for rear lane housing products.



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### 3.5 LYONS CREEK ENCLAVE PRECINCT

The Enclave lands are situated on the south side of Lyons Creek Road, at the eastern end of the LNDA. These lands are generally flat and back directly on to the Lyons Creek.



#### 3.5.1 Medium Rise Development

The following land uses should be developed within the Lyons Creek Enclave:

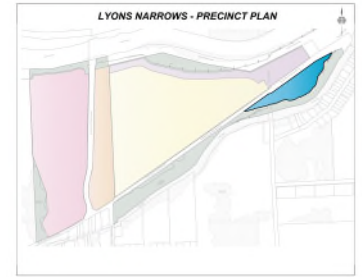
- Block Townhouses; *and*
- Apartments.

#### 3.5.2 Design Principles

- Block Townhouse dwellings should not exceed two (2) storeys in height.
- Apartment buildings should be small scale and not exceed 4 storeys in height.
- It is preferred that Townhouse Blocks and Apartments present the widest building wall towards the public realm or internal street rather than short side flankages.

#### 3.5.3 Accessibility and Connectivity

- Dwellings and buildings should provide dedicated pedestrian connections to sidewalks.



- Higher density land uses such as townhouses or apartments should be located in close proximity to public transit stops.
- If multiple blocks/ dwelling types are developed on the Enclave Lands, a shared/centralized pedestrian access from Lyons Creek Road is preferred.

#### 3.5.4 Parking

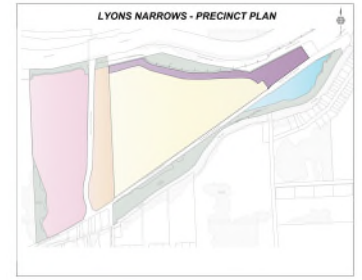
- Apartment developments should screen parking areas from view by placing them at the rear of the property, or underground.
- Parking along the sides of apartment buildings is acceptable, but should be screened with landscaping or other materials that positively contribute to the public realm.
- Townhouse dwellings should include garages suitably sized for vehicles,
- Visitor parking areas for rental or condominium townhouse developments should be located in a conveniently located central location, or evenly distributed throughout the site. These areas should be landscaped and screened from public view.

#### 3.5.5 Parkland and Trails

- Opportunities for the inclusion of private communal amenity areas, or public parkland is strongly encouraged along Lyons Creek.

## 3.6 WATERFRONT PRECINCT

Lyons Narrows is unique given its adjacency to the Welland River and Lyons Creek. Waterfront development opportunities will form some of the most valuable lots within the development area. It is important that



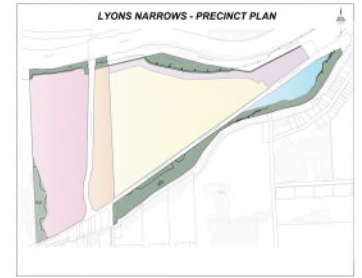
### 3.6.1 Design Principles

Development Proposed adjacent to the Welland River should have regard for the following guidelines:

- Ensure physical permeability between dwellings to provide public viewsheds to the water and natural environmental areas. Interior side yard setbacks should be increased where possible to provide breaks in the built form.
- The use of visually permeable fencing along rear lots lines is encouraged to avoid the creation of a visual barrier at the top of the river bank.
- Provision of a trail along, or in proximity to the Welland River is encouraged. The trail should have multiple pedestrian access points that are a minimum of 3 metres wide, with lighting.

## 3.7 NATURAL HERITAGE PRECINCT

Lyons Narrows contains and is adjacent to several evaluated natural heritage features and natural hazards that include provincially significant wetlands, significant woodlands, valleylands and floodplains.



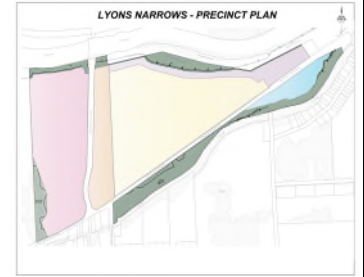
### 3.7.1 Design Principles

The long-term protection of these features is integral to the look, feel and appeal of Lyons Narrows. These features and their associated land use designations and zones are to be protected for the long term via the following Guidelines.

- Mitigation and protection measures prescribed in Environmental Impact Studies shall supersede these Guidelines, and serve as a minimum for feature protection.
- Trails and pedestrian network connections should be provided to natural areas, where possible and approved.
- The provision of trails or passive recreational opportunities within natural heritage features is supported (i.e. paths, footbridges, etc.)
- Private landscaping plans should ensure that native, or compatible species are proposed when located adjacent to identified natural heritage features.



- The provision of educational or informative placards or signage is encouraged within open spaces or along trails that are in proximity to key natural heritage features.



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