



Planning Justification Report

Zoning By-law Amendment and Consent

6645, 6655, and 6665 McLeod Road, Niagara Falls

For: McLeod Development Inc.

By: NPG Planning Solutions Inc.
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Date: February 5th, 2025

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1.0 Introduction

NPG Planning Solutions Inc. are planning consultants to McLeod Development Inc., “Owner” of the properties located at 6645, 6655, and 6665 McLeod Road (“Subject Lands”) in the City of Niagara Falls. The three (3) parcels that form the Subject Lands total approximately 0.42 hectares. NPG has been retained to provide professional planning advice on Applications for a Zoning By-law Amendment and Consent to facilitate the construction of three blocks of stacked townhouses for a total of 42 dwelling units and 47 parking spaces, and an easement for a shared driveway with the property to the east (6633 McLeod Road).

This Planning Justification Report (“PJR”) provides an analysis of the proposed development. It evaluates the appropriateness of Applications for a Zoning By-Law Amendment and Consent when assessed against policies in the Provincial Planning Statement (2024) (“PPS”), Niagara Official Plan (2022) (“NOP”), the City of Niagara Falls Official Plan (“City’s OP”), and the City of Niagara Falls Zoning By-law No. 79-200.

Overall, the Applications are consistent with the Provincial Planning Statement, and conform with the Niagara Region Official Plan and the City’s Official Plan. The proposed development focuses intensification along a Regional road, providing a range of housing options within an existing settlement area, while facilitating a high quality of design that has regard for the streetscape. The proposed development represents good land use planning and therefore we recommend that the applications for Zoning By-Law Amendment and Consent be approved.

Sections 5.1, 5.2, and 5.3 of this report provide an analysis of the proposed Applications within the Provincial and Regional planning policies. Section 5.4 of this report discusses the proposal’s conformance with the general intent and objectives of the City’s OP. Section 6.0 provides an analysis of the proposal’s urban design elements and Section 7.0 provides justification for approval of the Application for a Zoning By-Law Amendment.

2.0 Description of Subject Lands and Surrounding Area

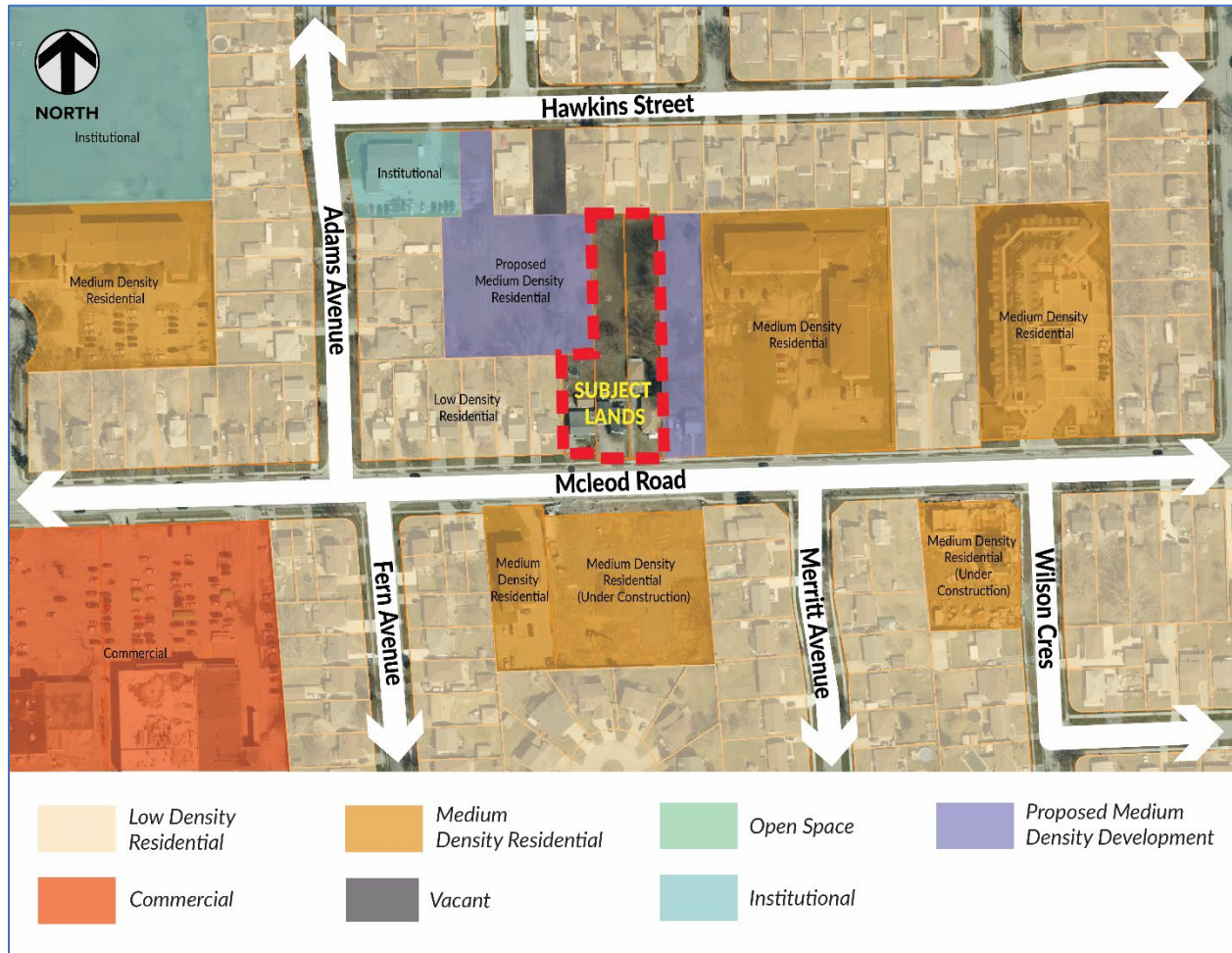
The Subject Lands are comprised of three parcels legally described as PT TWP LT 172 STAMFORD AS IN RO687481, NIAGARA FALLS; PT TWP LT 172 STAMFORD AS IN RO443613, NIAGARA FALLS; and PT TWP LT 172 STAMFORD AS IN RO250537, NIAGARA FALLS, and municipally known as 6645, 6655 and 6665 McLeod Road. The Subject Lands are irregular in shape and have a combined frontage of 46.33 metres, a depth of 112.67 metres, and a total area of 4,220.12 m². The Subject Lands contain the following buildings and structures that are proposed to be demolished as part of the proposed development:

Table 1 – Existing Structures on Subject Lands

Address	Buildings and Structures
6645 McLeod Road	One-storey dwelling with a detached garage
6655 McLeod Road	Two-storey dwelling
6665 McLeod Road	One-storey dwelling with a shed

The Subject Lands are generally flat and grassed with trees located throughout the site. Each parcel contains a single-detached dwelling fronting onto McLeod Road, a Regional road, with access currently provided via three separate driveways. 6645 and 6655 McLeod Road have a substantially greater lot depth than 6665 McLeod Road, with both parcels being over twice as deep as 6665 McLeod Road. Surrounding land-uses are noted as follows:

Figure 1 – Aerial Context



North: Residential – single-detached dwellings

East: Residential – anticipated stacked townhouse development at 6633 McLeod (zoning approved), and existing apartment building at 6601 McLeod

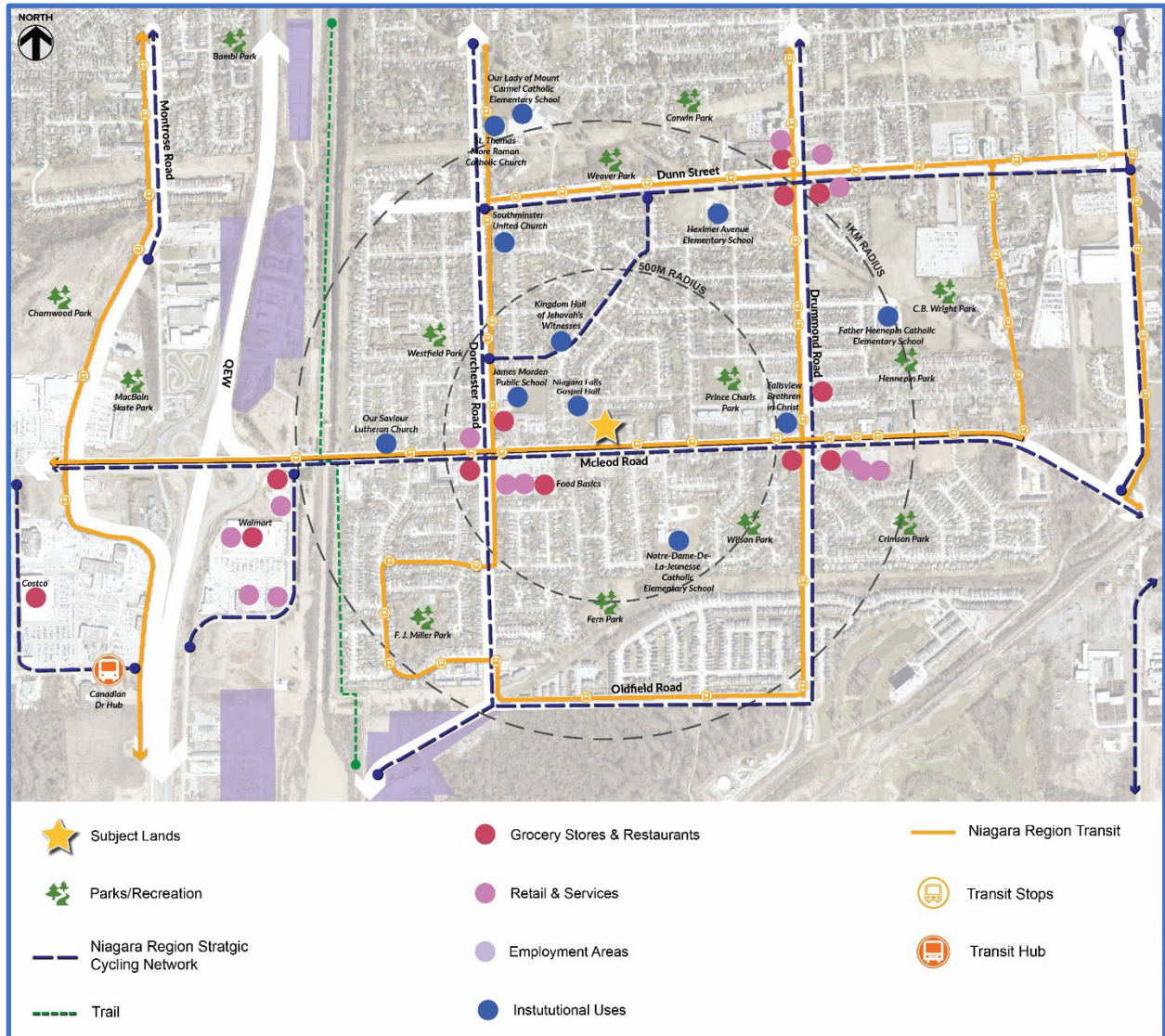
South: Residential – single-detached dwellings, and anticipated stacked townhouse dwellings at 6638 McLeod (under construction)

West: Residential – single-detached dwellings, and anticipated townhouse development at Habitat for Humanity Hawkins Street property (zoning approved)

The Subject Lands are immediately surrounded by a mix of low-density and medium-density residential uses, with single-detached dwellings abutting the Subject Lands to the north along Hawkins Street, and to the south and west along McLeod Road. Surrounding medium-density residential developments are comprised of both existing and anticipated developments, with existing 3-storey and 4-storey apartment dwellings located south and east of the Subject Lands along McLeod Road. Anticipated medium-density developments include a townhouse development (Habitat for Humanity Hawkins Street

property) directly west of the Subject Lands, a stacked townhouse development directly east of the Subject Lands (6633 McLeod Road), with which the easement for the shared driveway is proposed, and a stacked townhouse development to the south which is under construction.

Figure 2 – Community Amenities Map



The Subject Lands are located in the Drummond neighbourhood, with varying residential and commercial uses (see **Figure 2 – Community Amenities Map**). In addition to the residential uses are commercial and institutional uses within a 500-metre radius of the Subject Lands. These consist of various commercial uses such as grocery stores, gas stations, restaurants, and retail stores approximately 500 metres away at the intersection of McLeod Road and Dorchester Road, along with places of worship and schools. The proximity of various commercial and institutional uses to the Subject Lands provides existing and future residents with convenient access to a range of necessities for daily living.

Additionally, there are a range of mobility options in the neighbourhood that provide multimodal transportation opportunities and enhanced connectivity for area residents. The Subject Lands are well-served by existing public transit, with Niagara Region transit routes #112 and 203 on McLeod Road, along with other transit routes at the intersection of McLeod Road and nearby Dorchester and Drummond Roads that provide access to various hubs within the City. These transit routes include #101, 103, 111, and 211, and provide both local and regional connections to key destinations such as the Main Street Hub and Fort Erie Municipal Centre, facilitating greater access to the community and beyond. Furthermore, the Subject Lands are located approximately 1.1 kilometres away from the McLeod Road interchange with the Queen Elizabeth Way (QEW) Highway. The extent of McLeod Road between the QEW and Stanley Avenue is part of the Region's Strategic Cycling Network, which facilitates active transportation opportunities in the neighbourhood.

The Subject Lands are further contextualized by photos collected during a site visit. Please refer to the following photos for greater detail on the current state of the Subject Lands and the surrounding land uses (See **Photos 1-10**).



Photo 1 – View of southeast corner of Subject Lands looking west on McLeod Road.



Photo 2 – View of 6645 McLeod Road, looking north.



Photo 3 – View of 6655 McLeod Road, looking north.



Photo 4 – View of 6665 McLeod Road, looking north.



Photo 5 – View of southwest corner of Subject Lands looking east on McLeod Road



Photo 6 – View toward the south across McLeod Road from the Subject Lands of Stacked Townhouse Development.



Photo 7 – View of 6665, 6655 and 6646 McLeod Road. Photo looking north from south side of McLeod Road.



Photo 8 – View of rear yards of 6655 and 6645 McLeod Road, from mid-parcel, looking south. Shed belonging to 6655 McLeod Road in foreground.



Photo 9 – View toward the south of rear yards of 6655 and 6645 McLeod Road. Adjacent Habitat for Humanity site is bound by the fence on located on the right (west) side of photo.



Photo 10 – View looking toward the south, showing rear yard fence demarcating 6665 McLeod Road rear site boundary.

3.0 Proposed Development

McLeod Development Inc. is proposing applications for Zoning By-law Amendment and Consent to facilitate the development of three blocks of stacked townhouses with 42 dwelling units, 46 parking spaces, and an easement for a shared driveway with the property to the east at 6633 McLeod Road (“proposed development”). The Zoning By-law Amendment is required to address deficiencies in lot area, front yard depth, rear yard depth, interior side yard, parking space requirements, landscaped open space, amenity space, and width of drive aisle. The Consent Applications are required to facilitate the shared driveway easement, which is agreed upon with the neighbouring property owner. The Consent Applications No. B-2024-023 and B-2024-024 were conditionally approved at the January 28th, 2025, Committee of Adjustment meeting. A road widening of 2.15 metres for 6665 McLeod Road, and 5.2 metres for 6645 & 6655 McLeod Road, is required along the frontage of the properties.

The proposed development will have frontage on McLeod Road and will consist of three blocks of 3-storey stacked townhouse units. Block A consists of 6 rows of 18 stacked townhouse units fronting onto McLeod Road. Block B consists of 4 rows of 12 stacked townhouse units located in the interior of the property, facing the shared driveway and neighbouring property to the east. Block C consists of 4 rows of 12 stacked townhouse units located at the rear of the property, southerly facing the parking aisle (see **Appendix A**). The proposed development will deliver a residential density of 105 units per hectare with a lot coverage of 25.1%. One-bedroom, two-bedroom, and three-bedroom units with varying unit sizes are proposed, with a total of 12 one-bedroom units, 22 two-bedroom units, and 8 three-bedroom units across the proposed development.

The proposed housing type for all three blocks are stacked townhouse units. All stacked townhouse blocks will contain partially below grade, at-grade, and upper-level units with a height of 3-storeys or 10 metres. Each row within the block consists of 3 residential units.

The unit sizes and configurations vary between the three blocks. Block A provides for 1-bedroom units on the basement level and first-storey, ranging in size between 45 m² (483 ft²) to 50 m² (533 ft²), and a 2-bedroom unit on the second- and third-storey, with a square footage of 89 m² (961 ft²) to 92 m² (990 ft²). Block B provides for 2-bedroom units on the basement level and first-storey, ranging in size between 57 m² (613 ft²) to 63 m² (676 ft²), and a 3-bedroom unit on the second- and third-storey, with a square footage of 108 m² (1164 ft²) to 108 m² (1166 ft²). Block C provides for 2-bedroom units on the basement level and first-storey, ranging in size between 63 m² (683 ft²) to 70 m² (753 ft²), and a 3-bedroom unit on the second- and third-storey, with a square footage of 123 m² (1321 ft²) to 127 m² (1364 ft²).

Entrances to the units are provided through an external staircase at the architectural front of each stacked townhouse row. While the size of the units vary between the different blocks, the proposed layouts are consistent across the proposed development. The first and second unit in each row are located on the basement level and first-storey,

respectively. The third unit is located on the second- and third-storey of the row. Third-storey units feature balconies on the front building face for all blocks, as well as on the rear building face for Blocks B and C. All of the stacked townhouse are proposed to be clad with brick and wood siding, and feature balconies on the building faces, creating a consistent design style across the development.

Vehicular access is proposed from McLeod Road through a shared driveway with the neighbouring property to the east of the Subject Lands, municipally known as 6633 McLeod Road. The shared driveway will be constructed on both the Subject Lands and 6633 McLeod Road, with a 3.75-metre-wide aisle on each side for a total driveway width of 7.5 metres. The proposed shared access is an effective means of site circulation for both properties, as it provides direct access to parking areas in both developments, while reducing impervious surfaces and the impact of the driveway on the streetscape. Altogether, the shared access promotes an efficient site layout that unifies the design of both developments to overall strengthen the streetscape along McLeod Road.

Pedestrian sidewalks are currently provided along McLeod Road. The proposed Site Plan prepared by MXL Engineering & Associates Inc. includes well connected walkways providing direct access to the existing sidewalk from all proposed residential Blocks. The proposed development effectively facilitates pedestrian circulation throughout the site, with proposed walkways that provide safe and direct access from McLeod Road to Blocks A, B, and C, and through the parking areas. The walkways are augmented by landscaping. Landscaped strips and tree plantings abut the walkway between the main parking area, acting as both a buffer and an effective demarcation between pedestrian and vehicular space. Painted crosswalks are proposed to facilitate safe crossing for pedestrians.

The proposed development includes 46 surface parking spaces , including 2 accessible spaces, at a parking rate of 1.1 parking space per dwelling unit. Parking will be located to the interior of the development, with the main parking area between Blocks A and B and the remaining parking located in front of Blocks B and C. Landscaping is proposed to screen the main parking area from the pedestrian walkway, as well as from the rear of Block A. A waste collection area is provided adjacent to parking space 23.

Site lighting throughout the development consists of streetlights, LED directional path lights, and decorative lamp posts. Streetlights are proposed along the main driveway which ensures sufficient lighting for vehicles entering and exiting the site. LED directional lights are proposed at the entrances of the housing blocks, flanking the unit staircases. Decorative lamp posts are also employed throughout the development along landscaped areas and pedestrian pathways which effectively provides adequate lighting while enhancing the overall design of the development.

Proposed landscaping is outlined in the Landscape Plan prepared by James McWilliam Landscape Architect, dated August 2024. Proposed landscaping along the McLeod Road frontage contributes to the enhancement of the streetscape, with various plantings comprised of coniferous shrubs, ornamental grasses, perennials, and a row of deciduous

trees on a bed of sod. The front and side elevations of Blocks A, B, and C are further enhanced by various plantings of coniferous and deciduous shrubs, contributing to visual interest, while providing for privacy and buffering of the dwelling units. Deciduous shrubs and deciduous trees are proposed at the rear of Blocks B and C, respectively, which contributes to greater privacy. Lastly, timber perimeter fencing is proposed along the northern, western, and part of the eastern limits of the property. Altogether, these landscaping elements contribute to the proposed development’s unique design identity, provide a buffer between neighbouring properties, and contribute to enhanced privacy for both the proposed development and neighbouring properties.

Zoning By-law Amendment

The Subject Lands are presently zoned “Residential Low Density – Grouped Multiple Dwellings (R4)”. The Application for Zoning By-law Amendment proposes to rezone the Subject Lands to “Residential Low Density – Grouped Multiple Dwellings (R4)” with site-specific provisions from the General Provisions and Zone Requirements related to the following:

- Address zone deficiencies related to minimum lot area, minimum front yard depth, minimum rear yard depth, minimum interior side yard, minimum landscaped open space, and minimum amenity space; and,
- Permit reduced parking requirements and minimum perpendicular width of maneuvering aisle.

Consent Applications

The Consent Applications No. B-2024-023 and B-2024-024 for the shared driveway easement between the Subject Lands and neighbouring property at 6633 McLeod Road, were conditionally approved at the January 28th, 2025, Committee of Adjustment meeting.

3.1 Housing Affordability Threshold

Section 3.1 includes anticipated prices for the proposed dwelling units. Furthermore, it identifies how the proposed development fits with Provincial, Regional and City definitions of “affordable”.

3.1.1 Anticipated Sales Price

The applicant is anticipating sales prices ranging between \$650-700 per ft² for the units. For 1-bedroom units, the anticipated sales price is between \$313,950 to \$373,100. For 2-bedroom units, the anticipated sales price is between \$398,450 to \$693,000. For 3-bedroom units, the anticipated sales price is between \$756,600 to \$954,800. **Table 2** below outlines these anticipated prices in greater detail based on the housing block, unit, and unit size. It should be noted that these values are a range and are subject to change, depending on market conditions.

Table 2 – Proposed Unit Sales Price

Unit Type	Block and Unit	Unit Size (ft ²)	Proposed Sale Price	
1-Bedroom	Block A – Unit 1	505 ft ²	\$328,250 to \$353,500	
	Block A – Unit 2	483 ft ²	\$313,950 to \$338,100	
	Block A – Unit 5	500 ft ²	\$325,000 to \$350,000	
	Block A – Unit 6	533 ft ²	\$346,450 to \$373,100	
	Block A – Unit 7	533 ft ²	\$346,450 to \$373,100	
	Block A – Unit 8	500 ft ²	\$325,000 to \$350,000	
	Block A – Unit 11	500 ft ²	\$325,000 to \$350,000	
	Block A – Unit 12	533 ft ²	\$346,450 to \$373,100	
	Block A – Unit 13	533 ft ²	\$346,450 to \$373,100	
	Block A – Unit 14	500 ft ²	\$325,000 to \$350,000	
	Block A – Unit 17	483 ft ²	\$313,950 to \$338,100	
	Block A – Unit 18	505 ft ²	\$328,250 to \$353,500	
2-Bedroom	Block A – Unit 3	961 ft ²	\$642,650 to \$672,700	
	Block A – Unit 4	990 ft ²	\$643,500 to \$693,000	
	Block A – Unit 9	990 ft ²	\$643,500 to \$693,000	
	Block A – Unit 10	990 ft ²	\$643,500 to \$693,000	
	Block A – Unit 15	990 ft ²	\$643,500 to \$693,000	
	Block A – Unit 16	961 ft ²	\$642,650 to \$672,700	
	Block B – Unit 1	641 ft ²	\$416,650 to \$448,700	
	Block B – Unit 2	613 ft ²	\$398,450 to \$429,100	
	Block B – Unit 5	647 ft ²	\$420,550 to \$452,900	
	Block B – Unit 6	676 ft ²	\$439,400 to \$473,200	
	Block B – Unit 7	676 ft ²	\$439,400 to \$473,200	
	Block B – Unit 8	647 ft ²	\$420,550 to \$452,900	
	Block B – Unit 11	613 ft ²	\$398,450 to \$429,100	
	Block B – Unit 18	641 ft ²	\$416,650 to \$448,700	
	Block C – Unit 1	715 ft ²	\$464,750 to \$500,500	
	Block C – Unit 2	683 ft ²	\$443,950 to \$478,100	
	Block C – Unit 5	706 ft ²	\$458,900 to \$494,200	
	Block C – Unit 6	753 ft ²	\$489,450 to \$527,100	
	Block C – Unit 7	753 ft ²	\$489,450 to \$527,100	
	Block C – Unit 8	706 ft ²	\$458,900 to \$494,200	
	Block C – Unit 11	683 ft ²	\$443,950 to \$478,100	
	Block C – Unit 12	715 ft ²	\$464,750 to \$500,500	
	3-Bedroom	Block B – Unit 3	1164 ft ²	\$756,600 to \$814,800
		Block B – Unit 4	1166 ft ²	\$757,900 to \$816,200
Block B – Unit 9		1166 ft ²	\$757,900 to \$816,200	
Block B – Unit 10		1164 ft ²	\$756,600 to \$814,800	
Block C – Unit 3		1321 ft ²	\$858,650 to \$924,700	
Block C – Unit 4		1364 ft ²	\$886,600 to \$954,800	
Block C – Unit 9		1364 ft ²	\$886,600 to \$954,800	

	Block C – Unit 10	1321 ft ²	\$858,650 to \$924,700
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3.1.2 Defining Affordable

The City OP builds on the Niagara Region’s definition of “affordable” for the purposes of determining conformity and for establishing housing targets. The PPS and the NOP delineate the criteria for defining "affordable" either based on average household incomes (income based) or market rates (market based). For affordable housing meant for individual ownership, rates must adhere to the more stringent affordability criterion between the following two categories, be it income based, or market based.

Income Based: *housing for which the purchase price results in annual accommodation costs which do not exceed 30 per cent of gross annual household income for low- and moderate-income households; or*

Market Based: *housing for which the purchase price is at least 10 per cent below the average purchase price of a resale unit in the regional market area;*

Income Based (Criteria #1)

The City of Niagara Falls Housing Strategy (January 2022) summarizes the following average household income deciles in Niagara, which were adjusted for inflation from the 2021 Census as shown in **Table 3**:

Table 3 – Niagara’s Lowest Six Income Deciles

Income Level	Decile	Gross Annual Household Income in Niagara Region ¹	Affordable Ownership Rates ²
Low	1 st	Less than \$23,868	At or below \$124,200
Low	2 nd	\$23,869 to \$36,288	At or below \$188,784
Low	3 rd	\$36,289 to \$48,924	At or below \$254,664
Moderate	4 th	\$48,925 to \$64,800	At or below \$337,284
Moderate	5 th	\$64,801 to \$84,132	At or below \$438,660
Moderate	6 th	\$84,133 to \$103,572	At or below \$539,460

The anticipated selling price of the 1-bedroom units in Block A, and the 2-bedroom units in Blocks B and C are projected to fall within the 4th, 5th, and 6th income deciles which meets the criteria for moderate-income households. This represents 28 units of the total 42 units.

¹ City of Niagara Falls Housing Strategy, January 2022

² City of Niagara Falls Housing Strategy, January 2022

Market Based (Criteria #2)

Table 4 provides the average market sale price for townhouses in the regional market area citing data from the Canadian Real Estate Association. The PPS identifies that affordable market value rate should be at a minimum of 10% below that of the average market value rate in the regional market area.

Table 4 – Average Townhouse Market Sale Price in Niagara Region

Unit Type	Average Market Value Rate in Niagara Region ³	Affordable Market Value Rate in Niagara Region
Townhouse	\$640,000	\$576,000

The anticipated selling price of the 1-bedroom units in Block A and the 2-bedroom units in Blocks B and C are projected to fall within the affordable market value rate in the Niagara Region. This represents 28 units of the total 42 units.

3.2 Housing Impact Statement

The City's Housing Directions Study Phase 2: Housing Strategy ("Housing Strategy") was presented to City Council on March 22, 2022. Council endorsed the Housing Strategy and recently passed OPA 149 to include policies that implement the Actions that were identified in the Strategy. Key among these new housing policies is a set of criteria in which applications for an Official Plan Amendment, Zoning By-law Amendment, Draft Plan of Subdivision, or Draft Plan of Condominium must demonstrate on how the proposal will contribute to achieving the City's annual housing targets, especially regarding affordable housing.

This section of the report fulfills policy 4.4 (Part 1) of the City's Official Plan requiring a housing impact statement as follows:

Table 5 – City's OP Policy 4.4 Analysis

SECTION	POLICY	ANALYSIS
4.4 a)	<i>The proposed housing mix by dwelling type and number of bedrooms, as applicable;</i>	Three (3) blocks of stacked townhouses are proposed for a total of 42 units. These units are comprised of 12 one-bedroom units, 22 two-bedroom units, and 8 three-bedroom units.
4.4 b)	<i>How the proposal contributes to achieving the City's annual housing targets as outlined in Part 1, Section 4, Policy 4.8 a) and b);</i>	The proposed development will contribute 28 units to the annual target of 135 units as outlined in criterion (a) where each unit is constructed with a purchase or

³ <https://stats.crea.ca/en-CA/>

		rental price at or below the specified threshold for affordability, as per the criteria outlined in Niagara Region’s definition of affordable housing. As the proposed development is for ownership housing, it would not contribute to criterion (b).
4.4 c)	<i>The estimated rents and/or sales prices of the development, indicating where they are either above or below the threshold for affordable as defined by the Niagara Region;</i>	The anticipated sales price for the units are as follows: between \$313,950 to \$373,100 for 1-bedroom units; between \$398,450 to \$693,000 for 2-bedroom units; and between \$756,600 to \$954,800 for 3-bedroom units. The anticipated sales price for 1-bedroom units and 2-bedroom units in Blocks B and C meets the threshold of affordable as defined by Niagara Region, representing a total of 28 units. It is recognized that this rate is subject to change, depending on market conditions.
4.4 d)	<i>Where construction of the units is expected to occur in phases, information regarding the number of housing units that would meet Niagara Region’s definition of affordable to be provided per phase, where applicable; and,</i>	Construction is not anticipated to occur in phases.
4.4 e)	<i>The proposed legal and/or financial mechanisms to ensure the delivery of any proposed new affordable housing commitments, and mechanisms to retain the long-term affordability of units, where applicable.</i>	No legal or financial mechanisms are proposed.

SUMMARY

Based on the above analysis, the proposed development is anticipated to contribute income and market based affordability. Within the proposed development, 28 of the 42 units will be affordable to *low- and moderate-income households* (i.e., purchase prices will be less than 30% of gross annual household incomes), as well as being at least 10% below the average purchase price of a resale unit in the regional market area. With more than half of the proposed units anticipated to be affordable, the proposed development is expected to contribute to achieving the City’s affordable housing targets.

4.0 Supporting Studies Review

As per the Pre-Consultation Agreement dated July 18th, 2024, the following items are required for Zoning By-law Amendment and Consent Applications:

1. Planning Justification Report (PJR) and Housing Impact Statement (enclosed within the PJR)
2. Urban Design Brief (enclosed within the PJR)
3. Site Plan and Coloured Site Plan with Site Statistics/Zoning Matrix
4. Elevations
5. Floor Plans
6. Landscape Plans
7. Consent Sketch
8. Tree Inventory and Preservation Plan Report
9. Noise & Vibration Study
10. Functional Servicing Report (FSR)
11. Stormwater Management Report
12. Draft Zoning By-law Amendment

The supporting studies and plans have been submitted as supporting documentation for the proposed development and should be read and reviewed in conjunction with this PJR. These studies have been prepared by qualified professionals according to industry standards. A thorough review of each document has been undertaken in preparing this PJR and it is noted that no significant issues have been raised.

4.1 Urban Design Brief

An Urban Design Brief has been prepared by NPG Planning Solutions Inc. and has been included as part of the Planning Justification Report. The Urban Design Brief discusses the various design aspects of the proposed development and evaluates the proposed development's compatibility with the surrounding neighborhood and impacts on the streetscape, with relation to the relevant design policies and guidelines. The Urban Design Brief concludes that the proposed development is compatible with the surrounding neighborhood in terms of built form and architectural design, represents a high standard of design excellence, and will not have negative impacts on the streetscape. Please refer to Section 6.0 for the complete analysis.

4.2 Tree Inventory and Preservation Plan Report

A Tree Inventory Report and Preservation Plan (TPP) was prepared by Jackson Arboriculture Inc., dated August 26th, 2024, to identify trees on and adjacent to the Subject Lands that will be impacted by the proposed development. The TPP identified 37 trees currently located on the Subject Lands, within the road allowance, and on neighbouring properties within 6 metres. The report identifies that 32 trees are required to be removed to accommodate the proposed development, with the preservation of five trees with the use of appropriate tree protection measures. Tree preservation measures include the installation of tree protection fences, ensuring that there is no intrusion into the tree

preservation zone (TPZ), prohibiting the storage of machinery, construction debris, materials, waste, or any other items within the TPZ; pruning tree branches and roots by a certified arborist; and ensuring that a certified arborist inspects tree protection fencing prior to and after construction.

A Landscape Plan has been prepared by James McWilliam Landscape Architects that illustrates proposed tree plantings and overall landscaping in the proposed development as tree replacements.

4.3 Noise & Vibration Study

A Noise and Vibration Study was prepared by RWDI, dated November 20th, 2024, to assess impacts on the proposed development from nearby stationary and transportation noise sources, prepared in accordance with the Ministry of Environment, Conservation, and Parks' (MECP) NPC-300 Environmental Noise Guidelines and the Region's Traffic Noise Control Policy (PW5.NO1.0). The study identifies potential noise impacts from road traffic and recommends the following mitigation measures for the proposed development:

- provision for the installation of air conditioning in design of dwelling units;
- inclusion of noise warning clauses related to transportation sound levels at the building façade;
- including suite bedroom window glazing with sound isolation performance up to STC-28 for the south façade of Block A; and,
- including suite exterior balcony door with sound isolation performance up to STC-25 for the south façade of Block A.

Based on the results of the analysis, including the recommendations for mitigation measures, the Noise Study concludes that the proposed development is feasible to meet the applicable sound and vibration criteria.

4.4 Functional Servicing Report

A Functional Servicing Report ("FSR") was prepared by AM Engineering, dated January 2025, to identify how the proposed development will be serviced and connected to existing municipal water and sanitary infrastructure. The FSR concludes that there is existing infrastructure capacity to service the proposed development. The following outlines a summary of the FSR:

1. The properties are serviced by existing municipal water, sanitary, and storm services. A 525mm and 600mm sanitary sewers exist in the McLeod Road right-of-way. Additionally, there is an existing 200mm water main within the McLeod Road right-of-way.
2. The proposed development will be serviced by a 150mm sanitary sewer at a minimum 0.5% slope off the 525mm sewer on McLeod Road. The sanitary sewer has adequate capacity for the expected peak flow.

3. The proposed development will be serviced by a proposed 150mm water main connecting to the existing 200mm municipal water main. Furthermore, fire flow test results indicate that required flow of 150 L/s would be available at an estimated 40 psi.
4. The proposed development will be serviced by surface and underground drainage systems as per the Stormwater Management Report prepared by MXL Engineering & Associates Inc. The minor system comprises swales and storm sewers. Major flows shall be directed to McLeod Road.

4.5 Stormwater Management Report

A Stormwater Management Report was prepared by MXL Engineering & Associates Inc., dated February 3rd, 2024, to review how the proposed development will be connected to existing stormwater infrastructure as well as to outline how stormwater will be managed on-site. The Stormwater Management Report concludes that the proposed stormwater management system for the development will maintain the pre-development surface and ground water divides for all design storms up to and including the 100-year event of post-development, with water quantity controls and surface runoff controls through LID practices, a new storm pipe flow system, and a combination of underground storm sewer and storm structures. Erosion and sedimentation control measures are proposed to minimize the effects of erosion during the grading of the site.

5.0 Planning Policies

The purpose of this Planning Justification Report is to evaluate the appropriateness of advancing an application for an Official Plan and Zoning By-law Amendment in the context of the Provincial Policy Statement (PPS), Niagara Official Plan (NOP), and City of Niagara Falls Official Plan (City OP). This section provides an overview and discussion of these documents.

5.1 The Planning Act

Section 2 of the *Planning Act* identifies matters of provincial interest for which Council shall have regard for in carrying out its responsibilities under the *Planning Act*. Included in these matters are:

- (h) the orderly development of safe and healthy communities;*
- (j) the adequate provision of a full range of housing, including affordable housing;*
- (p) the appropriate location of growth and development;*
- (q) the promotion of development that is designed to be sustainable, to support public transit and to be oriented to pedestrians;*
- (r) the promotion of built form that,*
 - (i) is well-designed,*
 - (ii) encourages a sense of place, and*
 - (iii) provides for public spaces that are of high quality, safe, accessible, attractive and vibrant;*
- (s) the mitigation of greenhouse gas emissions and adaptation to a changing climate.*

Matters of provincial interest are articulated in Provincial, Regional, and City planning documents, and discussed throughout this Planning Justification Report. In summary, the Applications provide for an appropriate location for growth and development, including the provision of a full range of housing by permitting stacked townhouses that contribute to a range of housing and unit types, affordable to moderate-income households. The compact form and location of the development along a commercial corridor within proximity to existing transit routes supports public transit, walkability, and reduced automobile reliance, while making efficient use of an under-utilized property through intensification. Furthermore, the proposed easement for the shared driveway represents orderly and safe development, as it contributes to a cohesive streetscape reducing impervious surfaces and limiting access to a Regional road.

For these reasons, the Applications have regard for matters of provincial interest as considered throughout this report. Consistency and conformity with Provincial, Regional and City policies are discussed in-depth in the subsequent sections of this report.

5.2 Provincial Planning Statement (2024)

The Provincial Planning Statement (2024) replaces the Provincial Policy Statement (2020) and the Growth Plan for the Greater Golden Horseshoe (2019). The PPS sets the policy foundation for regulating land use in Ontario. It requires that developments make efficient use of land and services and supports opportunities for long-term economic prosperity.

5.1.1 Planning for People and Homes

The PPS provides the followings with respect to Planning for People and Homes under Section 2.1:

POLICY

2.1.4 *To provide for an appropriate range and mix of housing options and densities required to meet projected requirements of current and future residents of the regional market area, planning authorities shall:*

a) maintain at all times the ability to accommodate residential growth for a minimum of 15 years through lands which are designated and available for residential development; and

b) maintain at all times where new development is to occur, land with servicing capacity sufficient to provide at least a three-year supply of residential units available through lands suitably zoned, including units in draft approved or registered plans.

2.1.6 *Planning authorities should support the achievement of complete communities by:*

a) accommodating an appropriate range and mix of land uses, housing options, transportation options with multimodal access, employment, public service facilities and other institutional uses (including schools and associated child care facilities, longterm care facilities, places of worship and cemeteries), recreation, parks and open space, and other uses to meet long-term needs;

ANALYSIS

The Subject Lands are located in a neighbourhood consisting of residential and institutional uses, and are in proximity to various commercial uses and parks within the neighbourhood. Furthermore, there are two existing Niagara Region transit routes along McLeod Road, with bus stops approximately 120 metres or a 2-minute walk from the Subject Lands. Given this context, the proposed development will contribute to the achievement of complete communities in the neighbourhood, as well as residential growth and housing supply. The proposed development provides for greater housing choice and housing diversity through a mix of unit types and configurations. Its proximity to various commercial, institutional, and recreational uses along a Regional road with existing transit

contributes to walkable, healthy, and complete communities. Lastly, as demonstrated in the Functional Servicing Report prepared by AM Engineering, dated January 2025, there existing sufficient servicing capacity to support the proposed development.

5.1.2 Housing

The PPS provides the following policies with respect to housing under Section 2.2:

POLICY

2.2.1 *Planning authorities shall provide for an appropriate range and mix of housing options and densities to meet projected needs of current and future residents of the regional market area by:*

a) establishing and implementing minimum targets for the provision of housing that is affordable to low and moderate income households, and coordinating land use planning and planning for housing with Service Managers to address the full range of housing options including affordable housing needs;

b) permitting and facilitating:

1. all housing options required to meet the social, health, economic and wellbeing requirements of current and future residents, including additional needs housing and needs arising from demographic changes and employment opportunities; and

2. all types of residential intensification, including the development and redevelopment of underutilized commercial and institutional sites (e.g., shopping malls and plazas) for residential use, development and introduction of new housing options within previously developed areas, and redevelopment, which results in a net increase in residential units in accordance with policy 2.3.1.3;

c) promoting densities for new housing which efficiently use land, resources, infrastructure and public service facilities, and support the use of active transportation; and

d) requiring transit-supportive development and prioritizing intensification, including potential air rights development, in proximity to transit, including corridors and stations.

ANALYSIS

The proposed development will contribute to a range of housing options in the neighbourhood. The proposal is for stacked townhouse units with a range of one-, two-, and three-bedroom units of varying sizes and sales price, which provides greater housing choices in the neighbourhood to meet the needs of current and future residents. Furthermore, the Subject Lands are currently underutilized with three existing single-detached dwellings on deep lots – the proposed development represents intensification

that is efficient and orderly within the urban area of Niagara Falls by introducing residential development in proximity to transit and various commercial uses, schools, places of worship, and parks (see **Figure 2**). Altogether, the proposed development makes an efficient use of existing land, resources, infrastructure, and public service facilities, and supports the development of complete communities.

5.1.3 General Policies for Settlement Areas and Strategic Growth Areas

The PPS provides the following policies with respect to settlement areas and strategic growth areas under Section 2.3.1 and 2.4.1:

POLICY

- 2.3.1.1** *Settlement areas shall be the focus of growth and development. Within settlement areas, growth should be focused in, where applicable, strategic growth areas, including major transit station areas.*
- 2.3.1.2** *Land use patterns within settlement areas should be based on densities and a mix of land uses which:*
- a) efficiently use land and resources;*
 - b) optimize existing and planned infrastructure and public service facilities;*
 - c) support active transportation;*
 - d) are transit-supportive, as appropriate; and*
 - e) are freight-supportive.*
- 2.3.1.3** *Planning authorities shall support general intensification and redevelopment to support the achievement of complete communities, including by planning for a range and mix of housing options and prioritizing planning and investment in the necessary infrastructure and public service facilities.*
- 2.3.1.4** *Planning authorities shall establish and implement minimum targets for intensification and redevelopment within built-up areas, based on local conditions.*
- 2.4.1.1** *Planning authorities are encouraged to identify and focus growth and development in strategic growth areas.*
- 2.4.1.2** *To support the achievement of complete communities, a range and mix of housing options, intensification and more mixed-use development, strategic growth areas should be planned:*
- a) to accommodate significant population and employment growth;*
 - c) to accommodate and support the transit network and provide connection points for inter-and intra-regional transit; and*

d) to support affordable, accessible, and equitable housing.

2.4.1.3 *Planning authorities should:*

b) identify the appropriate type and scale of development in strategic growth areas and the transition of built form to adjacent areas;

c) permit development and intensification in strategic growth areas to support the achievement of complete communities and a compact built form;

ANALYSIS

The Subject Lands are located within the Delineated Built-Up Area and front onto a Regional road. Furthermore, the Subject Lands are located within the McLeod Road Intensification Corridor per the City's Official Plan, which is a Strategic Growth Area. The proposed development focuses growth in an area with existing municipal services, infrastructure, public service facilities, and transit, which makes an efficient use of land, resources, and infrastructure. As a form of intensification, the proposed development makes an efficient use of land by promoting greater housing choice and locating density along existing transit with connections City-wide and region-wide. Overall, the proposed development contributes to the achievement of complete communities.

5.1.4 Energy Conservation, Air Quality and Climate Change

The PPS provides the following policies with respect to energy conservation, air quality, and climate change under Section 2.9:

POLICY

2.9.1 *Planning authorities shall plan to reduce greenhouse gas emissions and prepare for the impacts of a changing climate through approaches that:*

a) support the achievement of compact, transit-supportive, and complete communities;

c) support energy conservation and efficiency;

d) promote green infrastructure, low impact development, and active transportation, protect the environment and improve air quality; and

ANALYSIS

The proposed development represents a compact built form that makes efficient use of land, services, and infrastructure in the City's built-up area. Its proximity to various commercial, institutional, and recreational uses and access to transit along McLeod Road supports a built form that is transit-supportive and contributes to the achievement of complete communities. The proposed development is a form of intensification that makes efficient use of underutilized lands and utilizes existing resources, which supports overall energy conservation and efficiency. Green infrastructure is promoted throughout the development, with existing trees to be retained and proposed landscaping to be used for

stormwater retention and infiltration. Furthermore, pedestrian walkways are proposed throughout the development with direct connections to the municipal sidewalk. The closest bus stops are located 120 metres away, with the commercial plaza at the intersection of McLeod Road and Dorchester Road, Prince Charles Park, and the Notre Dame Catholic Elementary School located within a 500-metres or a 5-minute walk of the proposed development. These conditions contribute to walkability and encourage the use of active transportation and public transit, representing development measures that improve air quality and protect the environment.

5.1.5 Sewage, Water and Stormwater

The PPS provides the following policies with respect to municipal servicing under Section 3.6:

POLICY

3.6.1 *Planning for sewage and water services shall:*

a) accommodate forecasted growth in a timely manner that promotes the efficient use and optimization of existing municipal sewage services and municipal water services and existing private communal sewage services and private communal water services;

3.6.2 *Municipal sewage services and municipal water services are the preferred form of servicing for settlement areas to support protection of the environment and minimize potential risks to human health and safety. For clarity, municipal sewage services and municipal water services include both centralized servicing systems and decentralized servicing systems.*

3.6.8 *Planning for stormwater management shall:*

a) be integrated with planning for sewage and water services and ensure that systems are optimized, retrofitted as appropriate, feasible and financially viable over their full life cycle;

b) minimize, or, where possible, prevent or reduce increases in stormwater volumes and contaminant loads;

c) minimize erosion and changes in water balance including through the use of green infrastructure;

d) mitigate risks to human health, safety, property and the environment;

e) maximize the extent and function of vegetative and pervious surfaces;

f) promote best practices, including stormwater attenuation and re-use, water conservation and efficiency, and low impact development; and

g) align with any comprehensive municipal plans for stormwater management that consider cumulative impacts of stormwater from development on a watershed scale.

ANALYSIS

The proposed development is a form of intensification that will make use of the City's existing municipal sewage and water services. Stormwater management for the proposed development is adequate to support the proposed development, and will result in improved post development conditions that meet the City's stormwater requirements and minimize erosion, per the Functional Servicing Report and Stormwater Management Report prepared by AM Engineering and MXL Engineering & Associates, respectively.

SUMMARY

Based on the foregoing, the proposed development is consistent with the general policies and intent of the PPS as it is proposed within an existing settlement area and provides an opportunity for intensification through infill development and the utilization of existing municipal services. The proposed development would contribute to a range of housing options in the neighbourhood, and is a compact and transit-supportive built form that supports the achievement of complete communities.

5.3 Niagara Official Plan (2022)

The Niagara Official Plan (NOP) is intended “to guide the physical, economic and social development of the Regional Municipality of Niagara”. It contains objectives, policies and mapping that implement the Region's approach to provide for managing growth, growing the economy, protecting the natural environment, resources and agricultural land, and providing infrastructure.

Table 6 below outlines the applicable designations for the Subject Lands.

Table 6 – NOP Schedules

NIAGARA OFFICIAL PLAN SCHEDULES (see Appendix B)	
Schedule B – Regional Structure	<i>Delineated Built-up Area</i>
Schedule J1 – Transportation Infrastructure	<i>On Regional Road 49 (McLeod Road)</i>
Schedule J2 – Strategic Cycling Network	<i>On Strategic Cycling Network</i>
Schedule M – Road Allowance Widths	<i>Regional Road 49 – McLeod Road from Oakwood Drive to Marineland Parkway: 30.5m</i>

5.3.1 Managing Growth and Intensification

The NOP provides the following policies with respect to growth and intensification:

POLICY

2.2.1 Managing Urban Growth

2.2.1.1 *Development in urban areas will integrate land use planning and infrastructure planning to responsibly manage forecasted growth and to support:*

a) the intensification targets in Table 2-2 and density targets outlined in this Plan;

Table 7 – Excerpt of Table 2-2 from NOP: Niagara Region Minimum Residential Intensification Targets by Local Municipality 2021-2051

Municipality	Units	Rate
Niagara Falls	10,100	50%

b) a compact built form, a vibrant public realm, and a mix of land uses, including residential uses, employment uses, recreational uses, and public service facilities, to support the creation of complete communities;

c) a diverse range and mix of housing types, unit sizes, and densities to accommodate current and future market-based and affordable housing needs.

e) built forms, land use patterns, and street configurations that minimize land consumption, reduce costs of municipal water and wastewater systems/services, and optimize investments in infrastructure to support the financial well-being of the Region and Local Area Municipalities;

f) opportunities for transit-supportive development pursuant to Policies 2.2.17, 2.2.2.18 and 2.2.2.19;

g) opportunities for intensification, including infill development, and the redevelopment of brownfields and greyfield sites;

h) opportunities for the integration of gentle density, and a mix and range of housing options that considers the character of established residential neighbourhoods;

i) the development of a mix of residential built forms in appropriate locations, such as local growth centres, to ensure compatibility with established residential areas;

k) orderly development in accordance with the availability and provision of infrastructure and public service facilities; and

l) mitigation and adaptation to the impacts of climate change by:

ii. where possible, integrating green infrastructure and low impact development into the design and construction of public service facilities and private development; and

iii. promoting built forms, land use patterns, and street configurations that improve community resilience and sustainability, reduce greenhouse gas emissions, and conserve biodiversity.

2.2.2 Strategic Intensification and Higher Densities

2.2.2.1 *Within urban areas, forecasted population growth will be accommodated primarily through intensification in built-up areas with particular focus on the following locations:*

b) areas with existing or planned public service facilities;

c) other locations with existing or planned transit service, with a priority on areas with existing or planned frequent transit service; and

d) local growth centres and corridors, as identified by Local Area Municipalities

2.2.2.5 *A Regional minimum of 60 per cent of all residential units occurring annually will be within built-up areas.*

ANALYSIS

The Subject Lands are located within the Region's Delineated Built-Up Area, within an intensification corridor on an arterial road, and in proximity to a major highway interchange. The proposed development will accommodate forecasted population growth through intensification, contributing to the City's density targets and providing for a compact built form and greater housing choice in an area predominantly comprised of low-density single-detached dwellings. The proposed development represents a form of gentle intensification that introduces greater density and a mix of built forms to the neighbourhood in a context-sensitive manner. Furthermore, the proposed driveway shared with the adjacent property to the east represents orderly development that seeks to minimize land consumption and potential conflicts with the Regional road. The shared access contributes to reduced impervious surfaces, allowing greater opportunities for landscaping throughout the development.

The proposed development is located along a transit route, with existing sidewalks on McLeod Road and proposed walkways from the development to the municipal sidewalk. The proposed development represents a transit-supportive development with an efficient land-use pattern, located in proximity to a range of uses that support daily living, which supports the achievement of complete communities. Furthermore, the proposed development will utilize existing municipal services, optimizing existing infrastructure capacity.

5.3.2 Housing

The NOP provides the following policies with respect to housing:

POLICY

2.3.1 Provide a Mix of Housing Options

2.3.1.1 *The development of a range and mix of densities, lot and unit sizes, and housing types, including affordable and attainable housing, will be planned for throughout settlement areas to meet housing needs at all stages of life.*

2.3.1.2 *Local Area Municipalities shall establish targets in Local official plans for an overall housing mix by density type and affordability based on consultation with the Region and the minimum affordable housing targets outlined in Policy 2.3.2.3.*

2.3.1.4 *New residential development and residential intensification are encouraged to be planned and designed to mitigate and adapt to the impacts of climate change by:*

a) facilitating compact built form;

b) incorporating sustainable housing construction materials or practices, green infrastructure, energy conservation standards, water efficient technologies, and low impact development.

2.3.1.5 *New residential development and residential intensification should incorporate universal design standards to meet housing needs at all stages of life.*

2.3.3 Planning Tools to Achieve Affordable and Attainable Housing

2.3.3.1 *The following tools will be considered to support the development of affordable and attainable housing:*

d) the inclusion of a mix of unit sizes in multi-unit developments to accommodate a diverse range of household sizes and incomes;

ANALYSIS

The proposed development introduces a type of housing density and built form that is compatible with the neighbourhood, adding to the housing stock diversity, choice, and affordability in the area. The proposed development represents a compact built form that makes an efficient use of land, with landscaping such as sod and tree plantings proposed throughout parking areas and the development, contributing to sustainable development practices. Direct pedestrian pathways are provided for throughout the site, connecting from the municipal sidewalk through to the parking areas and linking each block. Two accessible parking spaces are also proposed. These elements enhance accessibility throughout the site. Furthermore, the proposed development will contain a mix of one-bedroom, two-bedroom, and three-bedroom units. Varying unit sizes from 483 ft² to 1364 ft² are proposed and could accommodate a diverse range of household sizes and incomes. Within the proposed development, 28 units of the total 42 units are anticipated to contribute to affordable housing, as defined by the NOP.

5.3.3 Climate Change

The NOP provides the following policies with respect to climate change:

POLICY

3.5.3.1 *This section consolidates policy direction integrated throughout this Plan that supports the transition to net-zero communities that mitigate climate change. The Region will endeavor to:*

a) *support built form and land use patterns that use land efficiently and reduce transportation emissions by:*

i. requiring compact built form to minimize land consumption and increase the densities required to support transit ridership;

ii. promoting a mix of land uses to shorten commute journeys and support the creation of complete communities;

iii. planning to achieve minimum intensification and density targets which meet or exceed provincial requirements;

ANALYSIS

The proposed development is a form of intensification on underutilized properties within the Region's built-up area. The proposed development makes an efficient use of land and resources through a compact built form in an area that is transit-supportive and walkable. Commercial areas, parks, and schools exist in close proximity to the Subject Lands, which contributes to a mix of land-uses in the neighbourhood, encourages shortened commute journeys, facilitates active transportation, and overall contributes to the creation of complete communities.

5.3.4 Multi-Modal Transportation

The NOP provides the following policies with respect to multi-modal transportation:

POLICY

5.1.2.1 *Public transit will be a priority for transportation planning and investments to:*

a) *address the impacts of climate change;*

b) *assist in achieving social equity;*

c) *provide multimodal access to major transit station areas and other major trip generators identified in Policy 5.1.2.2. c); and*

d) *reduce dependence on the automobile.*

5.1.3.2 *Implementation of the Strategic Cycling Network as identified in the Region's Transportation Master Plan will be prioritized to advance the*

implementation of the Niagara Bikeways Master Plan as shown in Schedule J2.

5.1.5.1 *As conditions of the approval of a development application under the Planning Act:*

a) the Region may acquire land from the landowner required for the road allowance as identified in Schedule M, at no cost to the Region and free of all encumbrance, encroachments, and improvements unless otherwise agreed to by the Region; and

b) the Region shall be provided with a certificate of an Ontario Land Surveyor noting that all legal survey documentation on the widened road allowance is in place.

5.1.5.2 *The conveyance of land shall be required at no cost to the Region as a condition of the approval of a development application, beyond the designated road allowance widths identified in Schedule M, to accommodate items such as sight triangles, turning lanes, channelization, grade separations, traffic control devices, rapid transit, public transit facilities and rights-of-way, active transportation, cuts, fills and storm drainage requirements, as required to meet accepted engineering design standards. These do not require an amendment to this Plan.*

5.1.5.7 *Where new development for a sensitive land use is adjacent to a Regional Road, the Region will consider the need for a noise study or noise control measures to address traffic noise as per Provincial guidelines.*

ANALYSIS

The Subject Lands are located along the Niagara Region transit routes #112 and #203, and are within 500 to 700 metres of transit routes #101, 103, 111, and 211. Additionally, McLeod Road is part of the Region's Strategic Cycling Network. These existing transit and cycling connections provides alternate modes of transportation, promoting walkability, sustainability, and increased mobility for future residents of the proposed development.

As per Policy 5.1.5.1 and 5.1.5.2, a road widening of 2.15 metres for 6665 McLeod Road and 5.2 metres for 6645 and 6655 McLeod Road is required to be conveyed to the Region along the frontage of the Subject Lands, parallel to the existing right-of-way. This required widening is reflected in the site design, with additional landscaping conceptually proposed along the road widening to enhance the McLeod Road right-of-way. The road widening would be conveyed at the site plan approval stage.

The Noise Study prepared by RWDI dated October 8th, 2024, notes that subject to the recommendations for noise mitigation measures, the proposed development is feasible and meets the applicable sound and vibration criteria.

5.3.5 Infrastructure

POLICY

- 5.2.2.2** *Municipal water and wastewater systems/services are the required form of servicing for development in urban areas.*
- 5.2.2.4** *Prior to approval of development, the municipality shall ensure that required water and wastewater services and servicing capacity is available to support the development.*
- 5.2.5.1** *All new development and redevelopment in settlement areas must be provided with separate storm drainage systems or separate storm drainage connections.*

ANALYSIS

The Functional Servicing Report concludes that there is existing water and wastewater servicing available to serve the proposed development. Stormwater management details are included in the Stormwater Management Report.

5.3.6 Urban Design

The NOP provides for urban design policies in Section 6.2. Please refer to Section 6 – Urban Design Brief for an analysis of these policies in the context of the proposed development.

SUMMARY

Based on the foregoing, the proposed development conforms with the policies of the Niagara Official Plan by providing affordable housing and increased housing choice in a built-up area which supports a walkable and transit-oriented neighborhood and, subject to the analysis in Section 6 of this report, contributes to urban design excellence.

5.4 City of Niagara Falls Official Plan

The City of Niagara Falls Official Plan (“City OP”) provides policy direction for development of the City. The Official Plan recognizes the need for new and diverse housing options and is reflected in the McLeod Road Intensification Corridor policies.

Table 8 below outlines the applicable designations for the Subject Lands.

Table 8 – City’s OP Schedules

CITY OF NIAGARA FALLS OFFICIAL PLAN SCHEDULES (see Appendix C)	
Schedule A – Future Land Use	<i>Residential</i>
Schedule A2 – Urban Structure Plan	<i>McLeod Road Corridor Built Up Area</i>
Schedule C – Major Roads Plan	<i>Arterial Road: McLeod Road</i>
Schedule D – Community Planning Districts	<i>Community of Drummond</i>

5.4.1 Intensification

The City's OP provides the following policies with regard to intensification:

POLICY

- 3.5** *Intensification Corridors contain lands that front onto arterial roads and have the attributes conducive to supporting medium or high density residential redevelopment over the long term. Lands with frontage directly onto these corridors may be considered for residential use.*
- 3.7** *The McLeod Road intensification corridor provides a connection between the major commercial node at its interchange with the QEW and the tourist district commencing at Stanley Avenue. The corridor is characterized by single detached dwellings, mid-rise apartment buildings, a Minor Commercial node at Drummond Road and vacant lands in the eastern part of the corridor. Long term development along this corridor is envisioned as a mix of local serving commercial uses at the intersection of Drummond Road, and mid-rise residential buildings. In addition to the built form policies of Part 2, Section 1.15.5(iii), the following height and density provisions shall apply:*
- *building heights shall grade from a maximum of 8 storeys at the east end to a maximum of 4 storeys at the west end;*
 - *development density shall also have gradation from a minimum net density of 65 units per hectare at the west end and not exceeding 150 units per hectare at the east end.*

ANALYSIS

The Subject Lands front onto McLeod Road, a Regional and arterial road, and a corridor that has been identified for intensification within the City's Official Plan. The Subject Lands are within proximity to the commercial node identified at Drummond Road and are along existing transit routes, which make it an appropriate location to accommodate intensification. Furthermore, the proposed development is for medium density housing through the form of stacked townhouse units that are 3-storeys in height, with an overall density of 105 units per hectare. This is within the expected gradation of densities and heights along the intensification corridor. This introduction of gentle density to the neighbourhood increases housing diversity and choice, while ensuring compatibility with the existing neighbourhood and overall adding to the City's housing stock.

5.4.2 Housing

The City's OP provides the following policies regarding housing:

POLICY

- 4.1** *The City supports a range of housing uses and built form types, including housing that is affordable. To achieve this the City shall:*
- *Promote a greater diversity of housing types*
- 4.3** *Opportunities for a choice of housing including type, tenure, cost and location shall be provided to meet the changing needs of households throughout the Built-up Area and Greenfield Area. In order to achieve this goal, the City shall support the following:*
- 4.3.1** *Multiple unit developments, smaller lot sizes and innovative housing forms.*
- 4.3.2** *Development of vacant land, and more efficient use of under-utilized parcels and existing housing stock.*
- 4.3.3** *The full utilization and consolidation of properties to achieve larger scale and more comprehensive residential development.*
- 4.3.4** *Development of housing in conjunction with commercial developments in order to create walkable neighbourhoods.*
- 4.4** *Applications for an Official Plan Amendment, Zoning By-law Amendment, Draft Plan of Subdivision, and Draft Plan of Condominium shall include, as part of a complete application, the submission of a housing impact statement, either as a standalone report for large-scale projects, or as a section within a planning justification report for small-scale projects, demonstrating how the proposal implements the City's Housing Strategy. The housing impact statement shall include the following:*
- a)** *The proposed housing mix by dwelling type and number of bedrooms, as applicable;*
 - b)** *How the proposal contributes to achieving the City's annual housing targets as outlined in Part 1, Section 4, Policy 4.8 a) and b);*
 - c)** *The estimated rents and/or sales prices of the development, indicating where they are either above or below the threshold for affordable as defined by the Niagara Region and the City;*
 - d)** *Where construction of the units is expected to occur in phases, information regarding the number of housing units that would meet Niagara Region's and the City's definition of affordable to be provided per phase, where applicable; and,*
 - e)** *The proposed legal and/or financial mechanisms to ensure the delivery of any proposed new affordable housing commitments, and mechanisms to retain the long-term affordability of units, where applicable.*

4.6 *The City, in its review of subdivision/rezoning applications, will encourage provision of varying lot sizes, housing form and unit size in order to contribute to affordability.*

4.8 *Based on projections, it is expected that 20,220 new residential units will be built in the City between 2021 and 2051, or 674 new units on an annual basis. The City will aim to exceed the minimum targets for affordable housing established by the Niagara Region, which is set as 20% of all new rental housing built will be affordable and 10% of all ownership will be affordable.*

As such, the City has set an annual target of 40% of all new units meeting the definition of “affordable”. In this regard, the City will aim to achieve a minimum of 270 units to be built annually between 2021 and 2051 and beyond as affordable, with the following breakdown (the figure below provides an illustration of this for further clarity):

a) 135 units per year to be built with a purchase price or rental price at or below the identified threshold for affordable in accordance with the Niagara Region’s definition of affordable.

ANALYSIS

The proposed development is for stacked townhouse units with 1-, 2-, and 3-bedroom typologies in a range of sizes and configurations across Blocks A, B, and C. The proposed housing form and unit types contributes to the further housing diversity in the neighbourhood by providing for increased housing choice for current and future residents that may meet varying needs and affordability levels of those at different stages of life. As discussed in Section 3.2 of this report, the proposed sales prices of 28 units of the 42 units in the proposed development meet the definition of affordable. Furthermore, the proposed development supports increased housing supply and choice by making efficient use of underutilized lands. It leverages existing linkages to transit and proximity to various uses that contribute to daily living to propose residential development in a walkable neighbourhood. In order to facilitate the proposal, the development proposes consolidation of the three properties to fully utilize the lands and achieve an orderly and comprehensive development. The proposed easement for the shared driveway further represents a well-planned and integrated approach to development.

Please refer to Section 3.2 of this PJR for the Housing Impact Statement and detailed response to Policy 4.4 and 4.8 a) of the City’s OP.

5.4.3 General Residential

The City’s OP provides the following policies regarding the general residential land-use designation:

POLICY

- 1.1 *The predominant use of land in areas designated Residential shall be for dwelling units of all types catering to a wide range of households. Predominant uses shall include single detached and semi-detached dwellings, duplexes, triplexes, quadraplexis, townhouses, apartments, group homes and other forms of residential accommodation.*
- 1.5 *Building heights referred to in this Plan through the text and schedules are intended as a general guide. Consideration may be given in specific situations to allow suitable, well-designed developments that exceed these height guidelines through an implementing zoning bylaw amendment.*
- 1.7 *All residential development shall require proper and adequate municipal services. The municipality shall promote phased development to maintain logical, outward growth in residential areas in accordance with the policies of Part 3, Section 1.*
- 1.8 *Exposure of new residential development to less compatible land uses such as arterial roadways, highways or railway lines shall be minimized. In order to protect residential development from such uses, studies may be required and appropriate measures such as landscaping, berming or other buffering techniques may be required to determine impacts and recommend mitigation measures.*

ANALYSIS

The Subject Lands are designated Residential in Schedule A – Future Land-Use Plan of the City’s Official Plan and is intended for a wide range of residential uses per Policy 1.1. The proposed development is a permitted housing form that will accommodate a multiple unit development by making efficient use of an under-utilized property in a built-up area. While the surrounding area is predominantly comprised of low-density housing forms such as single-detached dwellings, there are other medium density housing forms such as stacked townhouse units and low-rise apartment buildings that are 3- to 4-storeys in height. The proposal is for a 3-storey stacked townhouse development which would contribute to the housing mix and variety in the neighborhood and provide for greater housing choice. Furthermore, the proposed development is in proximity to commercial uses along an existing transit route on McLeod Road, which promotes walkability and active transportation and contributes to the achievement of complete communities. The proposed development will utilize existing municipal services to optimize existing infrastructure. Lastly, as the Subject Lands front onto McLeod Road, an arterial road and a Regional road, a Noise Impact Study was completed by RWDI, dated November 20th, 2024, which notes that the proposed development will meet the applicable sound and vibration criteria subject to the implementation of mitigation measures recommended in the report.

5.4.4 Built Up Area

The City’s OP provides the following policies regarding the built up area:

POLICY

- 1.10** *It is recognized that opportunities exist throughout the Built-Up Area as shown on Schedule A-2 to create new housing units. Intensification, while maximizing the density of a given land area, shall be designed to integrate into the surrounding neighbourhood. The following policies are to be considered in the design of residential development, intensification and infilling and read in conjunction with the policies of PART 1, Sections 2 and 3:*
- 1.10.1** *The character of the existing neighbourhoods within the Built-up Area shall be retained. Accordingly, residential development, intensification and infilling shall blend into the lot fabric, streetscape and built form of a neighbourhood.*
- 1.10.2** *A gradation of building heights and densities will be encouraged together with sufficient horizontal separation distances between taller buildings and low rise dwellings in order to ensure a complementary arrangement of residential uses.*
- 1.10.3** *Generally, development within the Built-up Area should be at a higher density than what currently exists in the neighbourhood. A harmonious mix of single and multiple accommodation will be encouraged through the Built up Area so that at any one time a variety of housing types will be available suitable for different age groups, household sizes and incomes.*
- 1.10.5** *Single detached housing is the dominant housing form in existing residential neighbourhoods. Increasing the amount of various types of multiple residential accommodations is encouraged in order to provide for an overall mix of housing within all communities. The inclusion of various housing forms through subdivisions, intensification and infilling shall not be mixed indiscriminately, but will be arranged in a gradation of building heights and densities according to the following policies:*
- (ii)** *Stacked townhouses, apartments and other multiple housing forms with building heights of not more than 4 storeys can be developed to a maximum net density of 75 units per hectare with a minimum net density of 50 units per hectare. Such development should be located on collector roads and designed with a street presence that is in character with the surrounding neighbourhood. In addition, setbacks should be appropriate for the building height proposed and greater where abutting lands are zoned for single or semi-detached dwellings.*

ANALYSIS

The proposed development is a form of infilling and intensification on an under-utilized property in the City's built-up area. Although the neighborhood to the north is generally comprised of single detached dwellings, there are existing medium density residential developments in the neighbourhood comprised of stacked townhouse units and low-rise apartment buildings. The Subject Lands are adjacent to two proposed medium density developments and are across from an existing low-rise apartment building and a stacked townhouse development that is under construction. Furthermore, properties to the east, west, and south of the Subject Lands are zoned for medium density residential, whereas properties to the north are zoned for low density residential. The proposed development represents residential development that is compatible with the existing built form and streetscape.

Policy 1.10.5 further directs that a mix of housing types should be provided through intensification and infilling, subject to the applicable criteria in Policy 1.10.5 (ii). The proposed development is for 3-storey stacked townhouse units with a density of 105 units per hectare, desirably located along McLeod Road, a Regional and arterial road and an intensification corridor. Notwithstanding the density requirements outlined in 1.10.5 (ii), the proposed density of 105 units per hectare is permitted per the McLeod Road intensification corridor policies in Part 1, Section 3 of the City's Official Plan, which states in Policy 3.7 that 65 units per hectare permitted at the west end of McLeod Road and should not exceed 150 units per hectare at the east end.

The proposed development employs various design and landscaping measures to mitigate potential impacts of the proposed development on the surrounding uses, create visual interest, and enhance the streetscape. The proposed development is sensitively designed and sited to avoid impacts on surrounding built forms, maintaining a minimum 3-metre interior side yard setback and a minimum 6-metre rear yard setback for all stacked townhouse blocks. These setbacks are supplemented by timber perimeter fencing proposed along the northern, western, and a segment of the eastern boundaries of the Subject Lands, which altogether combine to enhance privacy for the proposed development and mitigate adverse impacts on adjacent properties. The proposed density is suitable as it is located along a Regional and arterial road which supports the existing transit route along McLeod Road and contributes to a lively streetscape. Furthermore, the proposed 3-storey height is consistent with surrounding uses, and the provision of appropriate setbacks will effectively mitigate impacts on adjacent properties and provide a transition in height and density.

Through the architectural and landscaping features, the stacked townhouse units present a visually cohesive design that is unique to the development, while enhancing the streetscape along McLeod Road. The existing streetscape features sidewalks on both sides of the street and landscaped front lawns on residential properties, with newer developments under construction such as 6638 McLeod Road exhibiting enhanced landscaping. The proposed development contributes to an enhanced streetscape through the use of exterior materials such as brick finish and wood panel finish with various

colours to create visually interesting facades. These various exterior materials visually delineate the different storeys while creating an attractive and non-monotonous design. The architectural design, coupled with tree and landscaped plantings along the property's frontage, contributes to an improved and attractive streetscape.

5.4.5 Municipal Servicing

The City's OP provides the following policies regarding municipal servicing:

POLICY

1.2.4 *Development within the urban area shall be accommodated on the basis of full municipal services including sewers, storm sewers, water services and improved roadways. Uses within the urban area shall connect to municipal water and sanitary services, if available. Outside of the urban area, Council shall cooperate with the Niagara Region in promoting individual sewage disposal systems which incorporate proven new technology achieving reduced volumes and/or improved quality of effluents.*

1.3.1 *It is required that all new development or redevelopment within the City be connected to and serviced by a suitable storm drainage system. Appropriate systems may include underground pipes, ditches, culverts, swales, man-made and natural watercourses, detention storage areas or any other storm water management system acceptable to Council, the Niagara Region, the Niagara Peninsula Conservation Authority, and other agencies.*

ANALYSIS

The proposed development will be serviced by full municipal services. As noted in the Functional Servicing Report prepared by AM Engineering, dated January 2025, there is existing water and wastewater servicing available to service the development. Stormwater will be managed through LID measures, a new storm pipe flow system, and a combination of underground storm sewer and storm structures.

5.4.6 Transportation Corridors

The City's OP provides the following policies regarding transportation corridors:

POLICY

1.5.2 *The corridors within the City's jurisdiction shall be of the width provided for in this Plan and should contain sidewalks and bicycle lanes of sufficient width to safely accommodate users.*

1.5.4 *Access ramps from private developments should be minimized and amalgamated with adjoining properties where possible.*

1.5.6 *Public transit routes shall be designed to service employment areas, intensification areas, nodes and intensification corridors. It is desirable for public transit services to be encouraged in proximity to higher density*

residential developments, areas of high employment concentration, major medical and social service centres, housing developments for people with special needs and social amenity areas such as the Niagara Parks and other attractions. Transit stops should be located within hubs and major public facilities. The development of inner-City bus rapid transit lines into high density employment areas is encouraged.

1.5.18.4 Arterial Roads - *include all roadways under the Region's and City's jurisdiction that are designed to accommodate large volumes of traffic between major land use areas in the City.*

Regional Arterial Roads are designed to accommodate the movement of large volumes of traffic and function as secondary highways and primary arterial roads. Design, road allowance width, use, alignment and access are regulated by the Regional Municipality of Niagara. Road widths vary from 20 metres to 42 metres.

City Arterial Roads accommodate two to four lanes of traffic and have a general road allowance width of 26 metres. Direct access to adjoining properties and on-street parking will be restricted as much as possible to enhance the free flow of traffic. The road allowance may accommodate transit routes with bus lay-bays and shelters and/or bicycle facilities such as bike lanes, shared use lanes and paved shoulders. The use of shared driveways to larger development projects will be encouraged in the urban areas. Regional and certain City roadways that function as arterial roads, primarily in the tourist core area, are subject to the policies described in the "tourist commercial roads" classification.

1.5.23 *The dedication of land for roads and rights-of-way improvements will conform to prescribed Provincial and Regional standards. Where lands are required for road construction or widening, such lands shall be conveyed to the appropriate public body as a condition of site plan control, consent to sever or plan of subdivision and when such road construction or widening is contemplated on a City-owned road within a five year time space. Road widenings, as identified in Policy 1.5.19, may be required to expand the width of the travelled portion of the roadway, or for servicing locations, including ditches and drains.*

1.5.31 *Council shall require the preparation of a noise study in accordance with Ministry of the Environment guidelines, where new residential or institutional development is proposed in proximity to major roads.*

1.5.33 *Where land for road widening is required as a condition of site plan control or lot creation, such land shall be acquired equally from both sides of the road as measured from the centreline wherever feasible. Where*

topography, existing or proposed development, utilities, or other constraints, necessitate a larger widening on one side, no more than 50 percent of the required widening shall be acquired through the planning application. Additional land may be acquired by other means.

1.5.34 The proposed widths of roads are as follows:

Road	From	To	Regional Arterial
McLeod Road	Oakwood Drive	Marineland Parkway	30.5 metres

ANALYSIS

The proposed development fronts onto McLeod Road, a Regional Road and a City arterial road. The proposed site access is from McLeod Road through a shared driveway with the development to the east. This proposed configuration will minimize potential traffic conflicts on McLeod Road and provide for a more efficient use of land that mitigates impacts on traffic and provide for enhanced safety. The Consent Applications No. B-2024-023 and B-2024-024 were conditionally approved at the January 28th, 2025, Committee of Adjustment meeting. Furthermore, there are existing municipal sidewalks on McLeod Road, with proposed pathways from the development that connect to the street to facilitate a pedestrian-oriented design.

The Subject Lands are located along a major roadway with a high average annual daily traffic count. As such, a Noise and Vibration Study was prepared by RWDI, dated November 20th, 2024, which assessed the potential noise impacts on the proposed development and potential mitigation measures. The noise study notes that the proposed development will meet the applicable sound and vibration criteria through the implementation of mitigation measures as recommended in the report.

Lastly, a road widening of 2.15 metres at the west end and 5.2 metres at the east end of the Subject Lands are required to be conveyed to the Region as part of this development. The road widening have been accounted for on the Site Plan prepared by MXL Engineering and Associates Inc., dated December 31st, 2024, enclosed with this submission.

5.4.7 Energy Resources

The City’s OP provides the following policies regarding energy resources:

POLICY

3.1.1 *The City shall encourage an energy efficient community where land uses are distributed to reduce travel needs, vehicle trips and subsequent energy use. A complementary arrangement and mix of uses which minimizes land use conflicts but also allows for integration shall be encouraged. This approach is advocated by the Future Land Use Schedule "A" and may be further refined through secondary plans. In order to achieve energy efficiency, the following shall be considered.*

- 3.1.1.1** *A compact and contiguous pattern of urban growth shall be promoted, including the intensified use of land and higher densities of residential development, where appropriate.*

ANALYSIS

The proposed development is a compact built form, located along an existing transit route and in walking distance to commercial uses and community facilities, which encourages walkability and active transportation. The location is appropriate for supporting a higher density given the existing commercial uses and transit route along McLeod Road, and its walkability contributes to energy efficiency and reduces reliance on automobiles.

5.4.8 Urban Design Strategy

Urban design policies are outlined in Part 3, Section 5 of the City's OP. The policies in this section discuss urban design elements as related to built form, height, massing, landscaping, and parking. Please refer to Section 6 – Urban Design Brief for a detailed analysis of these policies in the context of the proposed development.

5.4.9 Consents

POLICY

- 8.1.2** *Consents may be permitted for a minor boundary adjustment, easement or right-of-way.*
- 8.1.3** *Consents will only be permitted when the land fronts on a public road which is of an acceptable standard of construction.*

ANALYSIS

The Subject Lands front onto a public road, with the shared driveway contributing to more efficient and comprehensive development in the City's built-up area, while limiting access to and minimizing traffic impacts on a Regional road. Furthermore, the Consent Applications No. B-2024-023 and B-2024-024 for the shared driveway easement between the Subject Lands and neighbouring property at 6633 McLeod Road, were conditionally approved at the January 28th, 2025, Committee of Adjustment meeting.

SUMMARY

Based on the foregoing, the proposed development conforms with the policies of the City's Official Plan by facilitating intensification in the form of compact medium density housing along a Regional road, contributing to increased housing choice and greater housing diversity in the neighbourhood, making an efficient use of underutilized lands, utilizing existing municipal services, ensuring compatible development through architectural and landscaping design, and supporting active transportation and existing transit in a walkable neighbourhood.

6.0 Urban Design Brief

This Urban Design Brief aims to describe and elaborate how the proposed development's design elements are informed by good urban design principles and practices. It also discusses the various design aspects of the proposed development and provides a detailed explanation on how the proposed development is compatible with the surrounding neighborhood and addresses the relevant design policies and guidelines.

6.1 Site Contextual Analysis

The Subject Lands are located along the McLeod Road intensification corridor, with varying residential densities and built forms along the corridor between Dorchester Road and Drummond Road. The streetscape along this stretch of McLeod Road consists of primarily single-detached dwellings, low-rise apartment buildings, and stacked townhouse developments, with commercial uses located to the east and west at the intersections of Drummond Road and Dorchester Road, respectively. The streetscape character is categorized by existing sidewalks on both sides of the street, general landscaping in the front yards through a combination of grassed lawns and tree plantings, and a combination of driveway and garage parking. Notably, the low-rise apartment buildings and stacked townhouse developments along this corridor primarily feature parking in the rear or side yards, screened by landscaping.

The commercial nodes on the east and west ends of this corridor at the intersection of Drummond Road and Dorchester Road are located approximately 500 to 600 metres from the Subject Lands. Both nodes contain various stores and retailers that provide a number of essential shopping and services to the area. There are existing Niagara Region transit routes #112 and #203 on McLeod Road, along with other transit routes on nearby Dorchester and Drummond Roads, providing connectivity within the community and region. McLeod Road is part of the Region's Strategic Cycling Network.

These surrounding uses and existing context contributes to a neighbourhood context that is varied in nature, without a cohesive or distinct design character, albeit walkable with various available mobility options. The combination of varying residential uses, location along an intensification corridor, and proximity to two commercial nodes, presents a key opportunity for a compact and pedestrian-scale development. Given this context, it is essential that the proposed development provides an effective transition in built form, while enhancing the frontage on McLeod Road, and supporting walkable and active transportation-friendly neighbourhood.

Please refer to Section 2 of this report for the community context maps and photos of the Subject Lands.

6.2 Policy Context

6.2.1 City of Niagara Falls Official Plan (Office Consolidated 2024)

Urban design policies are provided in Part 3, Section 5 of the City's OP. The policies in this section discuss urban design elements as related to built form, height, massing,

landscaping, and parking. A summary and analysis of the applicable policies are discussed below:

POLICY

- 5.1** *New development, redevelopment and public works projects shall utilize building, streetscaping and landscaping designs to improve the built and social environment of the City and to enhance quality of life. Development should integrate and be compatible with the surrounding area including natural and cultural heritage features.*
- 5.1.1** *The design of new development and redevelopment shall specifically address height, setbacks, massing, siting and architecture of existing buildings in order to provide a compatible relationship with development in an area.*
- 5.1.2** *Development shall be designed and oriented to the pedestrian. As such buildings shall be set as close to the street as possible. Moreover, where development includes multiple buildings, the buildings should be deployed in such a manner that allows pedestrians to move between buildings with a minimum of interference from vehicular traffic. To this end, designated walkways through parking areas and to other buildings are to be provided.*
- 5.1.5** *Parking areas are to be minimized within the front yard of development sites. Parking shall primarily be located in the rear or side yards of development sites with sufficient landscaping utilized to create an effective buffer to abutting lands.*
- 5.1.7** *The number of access points onto arterial roads shall be minimized. Linked parking and driveway areas shall be encouraged. Access points shall be oriented toward major roadways.*

ANALYSIS

Through a combination of building and landscaping design, the proposed development positively contributes to the surrounding built environment and character of the neighbourhood. The stacked townhouse is a built form that is compatible with existing housing types in the area as it introduces gentle density along an intensification corridor in a manner that has regard for abutting low-density uses to the north. While the stacked townhouses are only 3-storeys in height, the siting of the housing blocks and design of the development provides for enhanced privacy through setbacks and landscaping elements such as timber perimeter fencing, sod, tree and shrub plantings, along the north and west perimeters of the property.

The orientation of the housing blocks recognizes the importance of an enhanced streetscape as well as compatibility with surrounding uses. Block A fronts onto McLeod Road, providing for a 1.5 metre front yard setback, and proposes pedestrian walkways

and enhanced landscaping along the frontage of the property. As Block A contains the greatest number of units of all the stacked townhouse blocks, the siting of this block onto McLeod Road, along with the reduced front yard setback and landscaping elements, creates a development that interfaces directly with the public realm, contributing to a vibrant and pedestrian-oriented streetscape. This pedestrian-oriented design is further enhanced by walkways that provide a direct and continuous path from the municipal sidewalk to each housing block and parking area. Where possible, these pedestrian walkways are flanked by landscaped strips, providing a buffer from the parking area.

Blocks B and C are sited to the interior of the development and are oriented in a manner that achieves a 6-metre setback from the rear of these blocks to the abutting property line. This provides adequate separation distance from neighbouring properties, and is further supplemented by the aforementioned landscaping measures. Furthermore, parking is located in the interior of the development and is buffered by landscaped strips, which reduces impacts on the streetscape and adjacent properties.

POLICY

- 5.3** *Landscaping and open space amenity areas can provide an opportunity to enhance the visual image of properties along the streetscape and should be incorporated in development projects to complement boulevard plantings. Landscaping can soften dominant building mass, screen noise and visual intrusion, shield against excessive wind and sun and provide various environmental benefits.*
- 5.3.1** *The orientation of landscaping within development sites should be toward public use areas, realizing the importance of the effective placement and maintenance of such landscaping in creating attractive amenity areas and entranceways. In addition, the City shall promote the substantial greening of the area intended for landscaping within development sites.*
- 5.3.3** *The size and extent of new plantings shall be appropriate for the mass and size of the building and surrounding area. Suitable tree types and plant species shall be selected having regard for their purpose, appearance, and resilience to conditions of the urban environment.*
- 5.3.4** *Landscaping, together with other design measures, can assist in mitigating the impacts of development on surrounding lands. Landscaping, where adjacent to buffer areas of natural heritage features, shall be designed to incorporate native species. The City shall encourage the utilization of adequate buffering, screening and other landscaping measures to ensure separation between potentially incompatible uses.*

- 5.3.5** *The City shall encourage the preservation and the incorporation of existing trees, vegetation, green areas and topography into the design and landscaping plans of proposed developments. Tree Preservation Plans may be required prior to any site alteration in compliance with PART 2, Section 11.*

ANALYSIS

Proposed landscaping throughout the development consists of tree, shrub, grass, and perennial plantings, along with timber perimeter fencing. A row of deciduous trees and ornamental grasses are proposed along McLeod Road, which contributes to an enhanced streetscape and acts as a buffer between Block A and the Regional road. Fencing is proposed along the northern and western property lines. Deciduous trees and shrubs act as an additional buffer between the rear face of Blocks B and C, and adjacent properties to the north and west.

The stacked townhouse blocks are augmented by various plantings, comprised of both deciduous and coniferous shrubs. These different shrub varieties flank the stacked townhouse blocks, demarcating the entrances to the units and creating attractive entryways. Furthermore, the development incorporates sod throughout, contributing to reduced impervious surfaces.

Although the Applications will require the removal of existing trees to facilitate the proposed development, the proposed development will preserve and protect 5 trees on the Subject Lands and on the adjacent western property, and will utilize various tree preservation measures such as the installation of tree preservation zones (TPZs).

POLICY

- 5.4** *Parking areas should be designed efficiently to minimize the extent of pavement and provide the opportunity for additional landscaping.*
- 5.4.1** *Green space and landscaping shall be interspersed throughout the parking area but not affect it's functioning and safety.*
- 5.4.2** *Traffic islands, paving materials, landscaping and lighting should be used to clearly distinguish between vehicle areas and pedestrian routes to provide safety and amenity.*

ANALYSIS

Throughout parking areas, landscaping strips are generally provided as a buffer between pedestrian walkways. This facilitates enhanced landscaping throughout the property while providing for ease of access between the housing blocks and parking areas. Furthermore, the proposed shared driveway contributes to reduced impervious cover, allowing for more efficient use of the development and greater landscaped coverage.

POLICY

- 5.7** *The City may prepare urban design plans or neighbourhood plans to implement the Urban Design Policies on specific areas. In the absence of such plans, reference should be made to Niagara Region’s Model Urban Design Guidelines.*

ANALYSIS

The City does not have an urban design plan or neighbourhood plan applicable to the Subject Lands. A review and analysis of the applicable Niagara Region Model Urban Design Guidelines are provided in Section 6.2.3.

6.2.2 Niagara Official Plan

While detailed urban design matters are regulated by the local municipality, the Niagara Official Plan provides general urban design direction in Section 6.2 of the NOP. These policies direct that developments along intensification corridors should ensure a pedestrian-oriented approach that contributes to vibrant streetscapes, and notes that the Region will promote well-designed buildings with high-quality streetscapes that contribute to neighbourhood character. A summary and analysis of the applicable policies are discussed below:

POLICY

- 6.2.1.3** *Areas strategically identified for intensification, including strategic growth areas, local growth centres and corridors, shall be designed to be pedestrian oriented with vibrant mixed-uses incorporating, where feasible, public gathering areas and public art.*
- 6.2.1.8** *The Region shall promote:*
- d)** *well-designed buildings, high quality streetscapes, and attractive public spaces that create neighbourhood character and strengthen community identity and diversity;*

ANALYSIS

The proposed development emphasizes its location along an intensification corridor through a pedestrian-oriented design that contributes to a vibrant streetscape. The decision to site the stacked townhouse block close to the street and provide enhanced landscaping and tree planting along McLeod Road fosters a positive relationship to the public realm. The stacked townhouse blocks feature a variety of exterior cladding materials which differentiate key elements of the units and provide visual interest and a cohesive yet non-monotonous design. Altogether, the proposed development represents a well-designed development that contributes to a high quality streetscape.

6.2.3 Niagara Region Model Urban Design Guidelines (2005)

As part of the Region’s Smart Growth agenda, the Niagara Region Model Urban Design Guidelines (NRMUDG) were adopted to implement ten Smart Growth principles for development and redevelopment throughout the Region, as follows:

1. Create a mix of land-uses
2. Promote compact built form
3. Offer a range of housing opportunities and choices
4. Produce walkable neighborhoods and communities
5. Foster attractive communities and a sense of place
6. Preserve farmland and natural resources
7. Direct development into existing communities
8. Provide a variety of transportation choices
9. Make development predictable and cost effective
10. Encourage community stakeholder collaboration

The Niagara Region Model Urban Design Guidelines further outlines guidelines for the public realm and private realm. Section 4A is most relevant to this Site Plan Application and outlines urban design guidelines for residential developments. Section 4a.1 sets out the following design principles for residential developments:

1. Positive Image
2. Context Sensitive
3. Housing Variety & Choice
4. Flexible & Adaptable
5. Environmentally Sustainable

The Region's Smart Growth principles emphasize the creation of complete communities through walkability, sustainability, and cost-effective development, whereas the design principles for residential developments delve into creating a high-quality architectural design that enhances the surrounding neighborhood, is appropriate for the immediate context, provides a variety of housing options for residents, fosters non-residential opportunities, and is environmentally sustainable.

A summary and analysis of the applicable guidelines are discussed below:

POLICY

4a.2 Building Variation and Density

- a) Housing variety should be achieved on each street and block as a means of strengthening neighbourhood character and identity. Repetition of house type, size and design (style, elevation, materials, etc) should therefore be avoided.*
- b) A full range of housing types (i.e., detached, semidetached, townhouse, apartments) should be provided to promote variety and diversity, and to address changes in market conditions.*

ANALYSIS

The proposed development contributes to housing variety in the area, as the stacked townhouse built form is not common in the neighbourhood, which is primarily comprised of single-detached dwellings. This contributes to a full range of housing types, which provides greater housing choice for current and future residents. Furthermore, the

proposed development leverages a distinctive architectural design through the use of different exterior materials, glazing, and placement of balconies that is unique from other stacked townhouse developments in the neighbourhood, which contributes to the neighbourhood character and identity.

POLICY

4a.7 Architectural Features

General

- b) Despite the use of various architectural styles, quality should be consistent and building materials and finishes should be complementary.*
- c) Consistent rhythms of similar but not identical details and architectural elements should be used to reinforce the streetscape and a strong neighbourhood image.*

Walls

- d) The front façade of dwellings and garage treatments should maximize the presence of the habitable building façade. A high standard of design, detail and variety of materials should be combined to create front building façades with a distinct street presence.*
- g) Facing materials including brick, stone, stucco and wood/metal siding are all acceptable. Lintels, cornices, quoins, dentils and other details are recommended to be incorporated within brick and stone walls to reduce the heavy effect of these materials.*
- h) Changes in the use of wall facing materials should occur at wall setbacks or projections, or to articulate the transition between the building base, middle and top.*

Windows

- j) Buildings facing or flanking a street, lane or open space should provide a generous amount of window openings to encourage strong visual connections between the private dwelling and public realm.*

Roofs

- w) A variety of roof shapes should occur in each residential block. However, roof forms should apply a consistent roofline in mass and height to adjacent buildings.*
- x) Roof materials/colours should complement the building materials and the proposed building design.*

ANALYSIS

The stacked townhouse blocks feature brick and wood siding finish in the façade design, which creates a consistent design style across the development. The front façade of the stacked townhouse units emphasize the habitable elements of the design through the

placement of windows and balconies that look out to McLeod Road, parking areas, and pedestrian walkways within the development. This contributes to a strong visual connection between the private and public realm and provides for passive surveillance of common spaces and the streetscape.

Various exterior materials are used to articulate the transition between the building base, middle, and top storeys. Wood siding finish is utilized to define the entryway to the dwellings, with the exterior material and projection from the building face providing a distinction between each row of units within the block and contributing to visual interest. The proposed design employs the use of brick to define the below-grade unit, which transitions to a different brick finish for the first two storeys. The upper storey is treated with a darker coloured wood siding finish to distinguish the top level from the lower storeys of the development. Furthermore, the proposed development provides for a consistent roofline through the use of a flat roof design that employs a mixture of wood siding finish and brick finish. The use of existing materials complements the overall building façade and contributes to a harmonious design. Altogether, these elements are utilized across all housing blocks, which reinforces a consistent rhythm and design style throughout the development.

POLICY

4f.2 Layout and Orientation

- c)** *Internal vehicular routes should be clearly defined by raised and curbed landscape islands planted with trees and low level vegetation. Internal drive aisles should be a minimum 6.0m wide. Parking bay dimensions should comply with municipal standards.*
- g)** *Designated handicapped and mobility impaired parking spaces should be located as close as possible to building entrances and be clearly identified by signs or markings.*

4f.4 Pedestrian Access

- a)** *Pedestrian walkways should be contiguous to main drive aisles opposite primary building entrances to enable safe and direct pedestrian movements.*
- b)** *An internal pedestrian walkway network should define visually and functionally smaller parking 'courts'.*
- c)** *Walkways should be a minimum of 3.5m wide, including a pedestrian zone of 1.5m wide and a landscaping zone of 2.0m wide.*
- d)** *Walkways should include pedestrian-scaled amenities wherever possible, such as benches, trash receptacles and lighting.*
- e)** *Drive aisle crosswalks should be signed and constructed of materials that are different to the drive aisle, such as interlocking brick paving.*

ANALYSIS

Access to the development is provided through a shared driveway with 6633 McLeod Road that is 7.5 metres wide. Off-street surface parking is proposed, located interior to the development and comprised of various parking areas. These parking areas are situated behind Block A and as rows along the front and side of Blocks B and C. Two accessible parking spots are provided adjacent to a pedestrian walkway, which provides ease of access to Block B as well as connecting walkways throughout the development. Internally, the parking areas are connected through drive aisles that are 6 metres wide, with a direct and connected set of pedestrian walkways contiguous to the drive that facilitate safe and efficient movement throughout the development.

POLICY

4f.3 Landscape Buffers

- a) *High quality landscaping treatments should be used to define site boundaries, provide buffers between adjoining developments, and screen storage and utility areas.*
- b) *The property setback of all parking areas should provide a landscaped area a minimum of 3.0m wide.*
- c) *Trees at the perimeter of parking areas should be planted every 6 to 9.0m on centre.*
- d) *To ensure opportunities for surveillance from adjacent areas, perimeter hedge and shrub screening should not exceed 1.0m in height.*
- e) *Selection of plant materials should consider the following:*
 - *year-round appearance;*
 - *seasonal variety;*
 - *hardiness & resistance to disease;*
 - *maintenance requirements; and*
 - *tolerance of plant materials to salt and urban conditions.*

4f.5 Internal Landscaping

- a) *Internal landscaping elements should define visually and functionally smaller parking 'courts' and reduce the overall impact of surface parking areas.*
- b) *A landscaped island should be located at each end of every parking aisle. Landscaped islands should be a minimum width of 2.5m wide and include one tree per parking row.*
- f) *Where possible, internal landscaping should incorporate existing vegetation and significant tree planting.*

4g.6 Solar Orientation

- g) *Landscape plans should use deciduous street trees and on-site trees where these trees will grow to shade windows of residential structures. Such trees provide shade and help reduce temperatures inside adjacent units during the warmer months and shed their leaves to allow sunlight and better heat penetration during cooler months.*

ANALYSIS

Landscaped treatments throughout the proposed development are utilized to define edges of parking areas, screen parking and waste collection, and shield enhance the housing blocks. Drive aisles and parking areas are generally flanked by landscaped strips that define the edges of the parking areas, which also provide an additional buffer between pedestrian walkways, property boundaries, and the housing blocks. Perimeter fencing is utilized along the northern and western property lines, which effectively screens parking and waste collection areas from the neighbouring property.

Furthermore, the front and side facades of the housing blocks feature various low height shrub plantings that enhance the facades of the development, while providing for passive surveillance of the interior parking areas. Different varieties of trees are proposed, with a row of deciduous trees proposed in front of Block A and along the rear of Blocks B and C. In addition to the proposed tree plantings, two existing trees on the western property boundary will be preserved as part of the development. These existing and proposed tree plantings will contribute to environmental sustainability and energy conservation on the site, providing for shade and reduced temperatures, as well as sunlight and heat penetration for the stacked townhouse units throughout different seasons of the year.

6.3 Design Goals & Principles

Based on the spatial analysis and discussion of the Subject Lands' in Section 6.1 of this report, as well as consideration of the applicable design objectives and guidelines in the City's Official Plan and the Region's Official Plan and Model Urban Design Guidelines, a number of design goals and principles have been established to guide the conceptualization and implementation of the proposed development.

The following design goals and their respective principles for the proposed development have been selected to:

- Incorporate designs that are compatible and appropriate for the surrounding neighborhood while providing visual variety and adding to the diversity of the built form:
 - Generating visual interest through varying height, setbacks, and architectural design elements; and,
 - Respecting privacy of neighboring properties through appropriate setbacks and landscaping design.
- Promote a safe, walkable, and pedestrian-oriented development:
 - Maintaining pedestrian connections and walkways throughout the development; and,

- Providing amenity areas and opportunities for natural surveillance.
- Contribute to a vibrant and pleasant streetscape experience along McLeod Road:
 - Enhancing the frontage along McLeod Road through façade treatments and landscaping; and,
 - Reducing the visual impacts of parking and service areas on the public realm and adjacent land-uses through landscaping and architectural design.

6.4 Design Response & Evaluation

This section discusses the various design aspects of the proposed development, particularly, “Site Design”, “Built Form Massing”, and “Building and Architectural Design”. Within these overarching design components, details related to pedestrian access and circulation, height, setbacks, landscaping, parking, architectural treatment, and more are also examined. It should be noted that there may be overlapping discussions regarding these various design categories. The discussion of these design elements are assessed based on how they implement the design goals and principles outlined in Section 6.3 above.

6.4.1 Site Design

The overall site design takes into consideration the Subject Lands’ location along McLeod Road, the existing municipal sidewalks, access to transit, and proximity to various commercial uses at the intersections of Drummond Road and Dorchester Road. These considerations have guided the design of the site by putting forth a compact and pedestrian-oriented development that contributes to an enhanced frontage along McLeod Road.

The design orients Block A, the housing block with the greatest number of units, to front onto McLeod Road with a proposed front yard setback of 1.5 metres. The front façade of the housing blocks feature entryways to the dwelling units, along with large windows and balconies across the various units. These habitable design elements and siting of Block A as close to the street as possible contributes to a strong street presence, ensuring a high visibility from McLeod Road. Furthermore, the glazed windows and balconies create transparency and a direct connection between the public and private realm, contributing to the perception of “eyes on the street”, which fosters passive surveillance of the public realm, a factor which is crucial to creating safe, vibrant, and pedestrian-oriented streets.

The front façades of the housing blocks are enhanced by various shrub plantings, framed between the staircases in each housing row, which fronts onto a pedestrian walkway (see **Figure 3**). The side facades of Blocks A and B are flanked by deciduous shrubs, contributing to enhanced design. High quality landscaping is further utilized to carefully treat the interface between the private and public realm. The transition between the Subject Lands and the street is facilitated by additional landscaping along the frontage, with deciduous trees interspersed with rows of ornamental grasses and perennials. These plantings have a dual purpose in contributing to an enhanced streetscape while further screening Block A from the roadway.

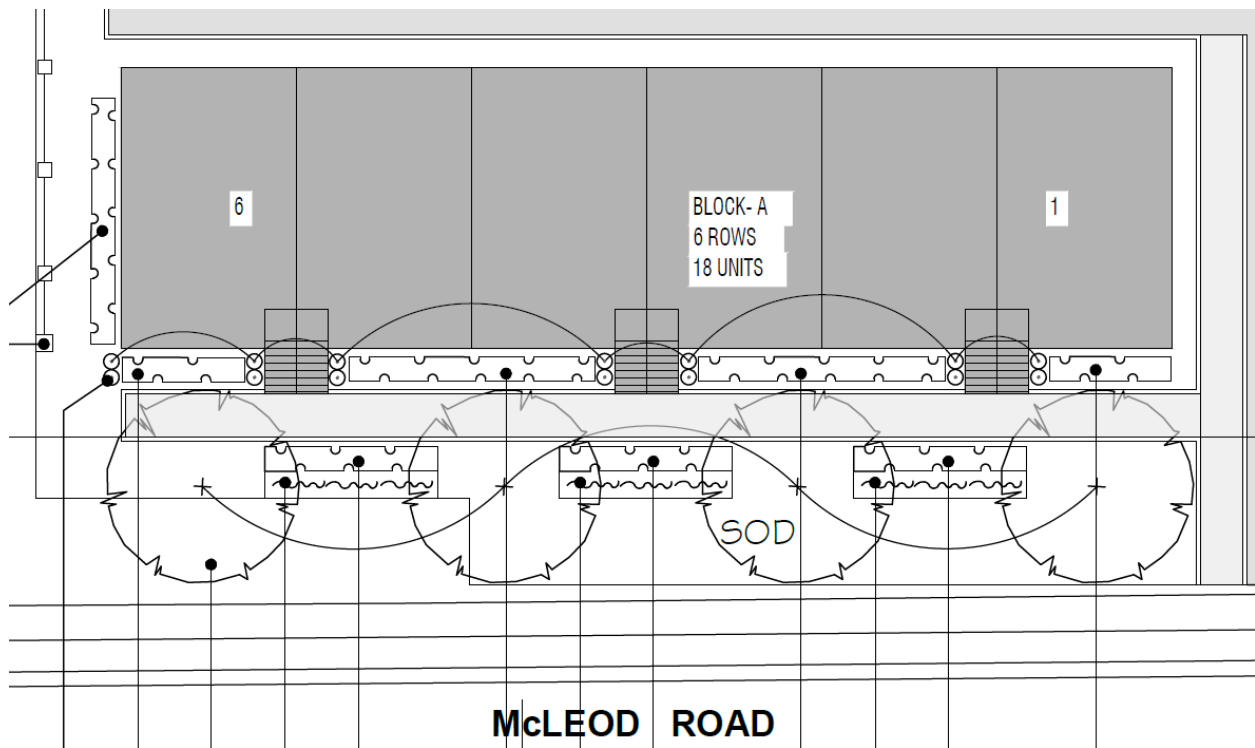


Figure 3 – Excerpt of Landscape Plan

Blocks B and C are located behind Block A, which can be accessed through an internal driveway and a network of pedestrian walkways. The pedestrian walkways provide access from the municipal sidewalk to the Block A unit entrances, which links together the interior parking areas as well as Blocks B and C. This design promotes pedestrian access and circulation throughout the development, facilitating safe and direct connections between the housing blocks, parking areas, and the street.

Parking and servicing areas are located to the interior of the development, screened by the housing blocks as well as landscaping and timber perimeter fencing. The main parking area is located behind Block A, out of sight when viewed from the street while remaining in close proximity to both Blocks A and B. Parking areas along the front and side of Blocks B and C provide for direct connections to the pedestrian walkway and housing blocks, with two accessible parking spaces and an accompanying access aisle located along Block B. Additionally, the proposed development's waste collection area is located in between the western property line and parking space 23. The waste collection area is adjacent to a 6-metre width drive aisle, and maintains a 2.7-metre and 2.3-metre setback to the north and west property lines, respectively. The waste collection area is further screened by 1.8-metre height timber perimeter fencing. The configurations of the parking and servicing areas ensures the reduction of visual impacts on the dwelling units, streetscape, and adjacent properties, while providing for logical vehicular circulation to access these areas.

The proposed development carefully incorporates site lighting in a strategic manner. Streetlights are the primary lighting fixture to illuminate the main driveway throughout the

development, whereas LED directional path lights and decorative lamp posts are employed in front of the housing blocks and along pedestrian walkways and landscaped areas. These lighting fixtures provide pedestrian-scaled lighting that facilitate safety and overall enhance the development's design.

Overall, the site design emphasizes the importance of enhancing and improving upon the character of the streetscape through a high quality of design and landscaping, providing for safe and efficient pedestrian circulation, and reducing negative impacts on the housing blocks and neighbouring properties.

6.4.2 Built Form Orientation and Massing

The Subject Lands are surrounded by varying built form contexts, with single-detached dwellings to the north and west and stacked townhouse dwellings that are proposed and under construction to the east and south, respectively. While this context consists of varying densities, the low-rise character is a consistent attribute in the immediate neighbourhood. This context is key in informing the design approach of the proposed development, which strives to mitigate massing impacts on adjacent properties, ensuring compatibility with the existing neighbourhood, and contributing to a vibrant and pedestrian-scale streetscape.

The massing and siting of the housing blocks on the Subject Lands takes into account the location of existing dwellings on adjacent properties, as well as the property's location along an intensification corridor. As previously discussed in Section 6.4.1, Block A is sited to face McLeod Road, oriented as close to the street as possible which creates a strong connection between the proposed development and the public realm. A 2.77-metre setback is proposed between Block A and the western boundary, a setback that is consistent with the existing side yard setback of 3 metres. As the side facades of the housing blocks do not feature windows or balconies, this orientation reduces overlook concerns with adjacent properties.

Block B is located in the middle of the development, oriented to face the drive aisle and neighbouring stacked townhouse development to the east at 6633 McLeod Road. This siting orients Block B away from the single-detached dwelling at 6675 McLeod Road, with a 6-metre setback from the rear façade to western property boundary, contributing to mitigation of potential noise and privacy concerns. A row of eastern redbud plantings 2-metres in height are proposed along the rear yard of Block B, along with an existing tree on the western property line that will be retained. Block C is similarly sited to the rear of the development, facing the internal drive aisle and maintaining a 6-metre setback from the rear façade to the northern property boundary. Tree plantings consisting of sugar maple and red maple trees are proposed along the rear of Block C. The orientation of these housing blocks are supplemented by appropriate setbacks, 1.8-metre-tall timber perimeter fencing, and proposed tree plantings along the side and rear facades of the housing blocks. The orientation and additional design measures positively contributes to the streetscape, while ensuring that visual impacts and privacy concerns on neighbouring properties are mitigated.



Figure 4 – Rendering of Block B

With the neighbourhood’s predominantly low-rise nature, the 3-storey height and massing for the proposed development is compatible with the existing neighbourhood character. Each housing block has an overall height of 10 metres, which contributes to a massing that appropriately addresses the scale of the neighbourhood and properly frames the streetscape. While Block A is the longest housing block in the proposed development, the massing is proportionately broken up by the use of wood siding finish to avoid visual monotony. The proposed height and massing of the housing blocks represent an appropriately scaled development that maintains a consistent street wall, contributes to a pedestrian-scaled development, and is compatible with the existing neighbourhood.

The overall built form orientation and massing ensures that the housing blocks are sited to reduce negative impacts on neighbouring properties, while contributing to a pedestrian-scale development that reinforces the street wall and is compatible with the existing neighbourhood.

6.4.3 Building and Architectural Design

The following section discusses the building and architectural design of the proposed development, exploring how the various building elements contribute to the overall design.

The building entrances for the housing blocks are prominently located on the building face and are sited to face either the public street or the internal drive aisles. The entrances to these residential units feature a staircase for the upper- and lower-level units, with the separate unit accesses framed by wood siding finish above, clearly delineating the unit

entrances. The use of wood siding finish above the unit entrances complements the use of other cladding materials on the façade while emphasizing the entrances. Balconies on the top storey provide visual interest throughout the housing blocks. These building entrance designs establish a clear but distinct pattern throughout the development (see **Figure 5**).

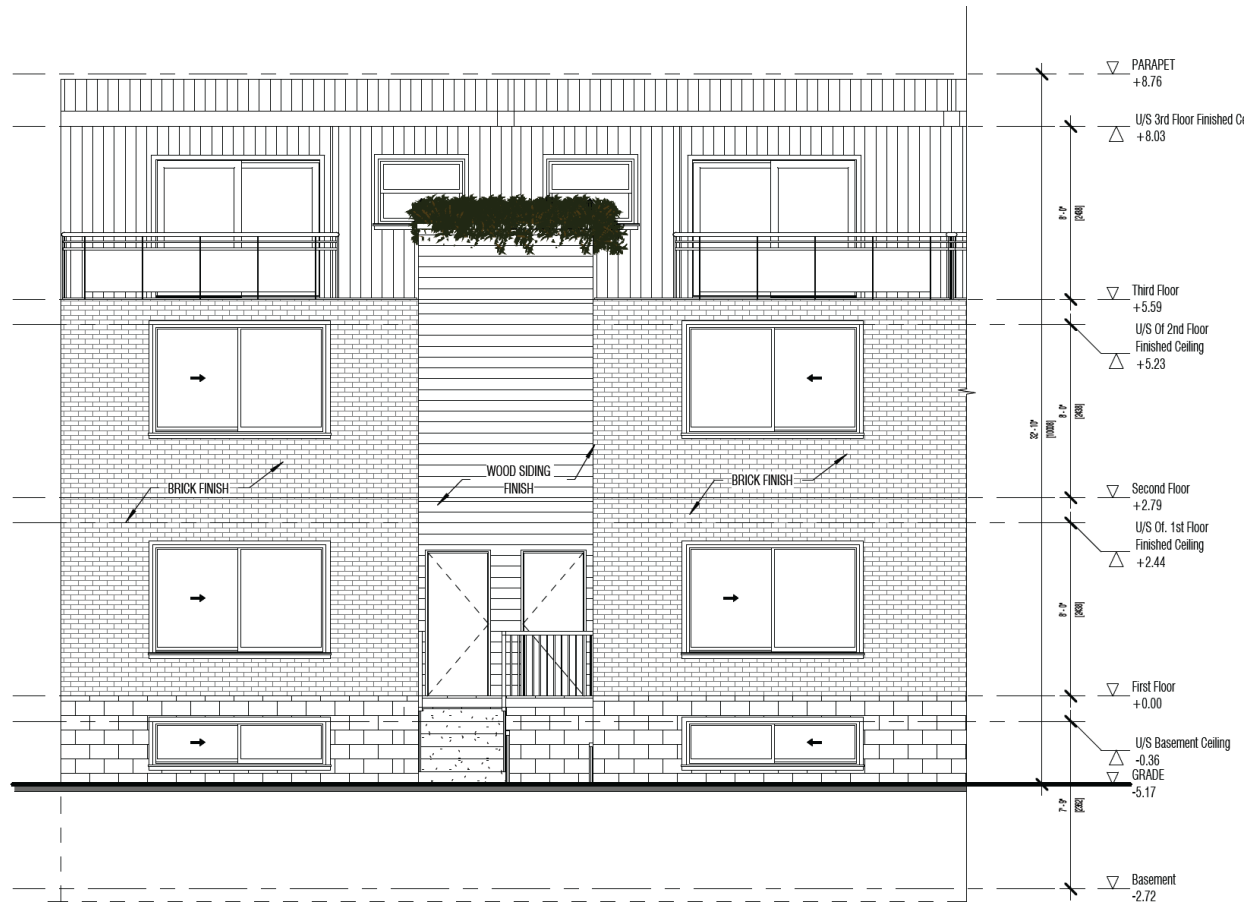


Figure 5 – Block C Front Elevation

The proposed development incorporates high-quality architectural detailing that achieves a pedestrian-scale design and a distinct design character. All of the housing blocks feature material and texture changes between the different levels in order to articulate the visual distinction between the storeys and emphasize key building elements. Notably, the lower-level units feature a brick finish in a different pattern than the first two storeys of the development. The top-level of the development is further treated with wood siding finish in a darker colour and different pattern than the wood finish above the unit entrances, and features balconies along the front and rear façades which overall contribute to visual interest across the housing blocks.

Another key architectural detail is the function and orientation of the wood siding finish above the unit entrances. This feature is oriented vertically and projects slightly from the building face, contrasting with the rest of the façade design. The use of material, colour, orientation, and articulation is an effective method in delineating the housing row in each

block. These changes in material and horizontal design elements establish a clear rhythm that avoids monotonous and “slab” facades, contributing to a reinforced street wall. The housing blocks are distinct in design with a consistent use of materials that combine to create a unifying character for the development.

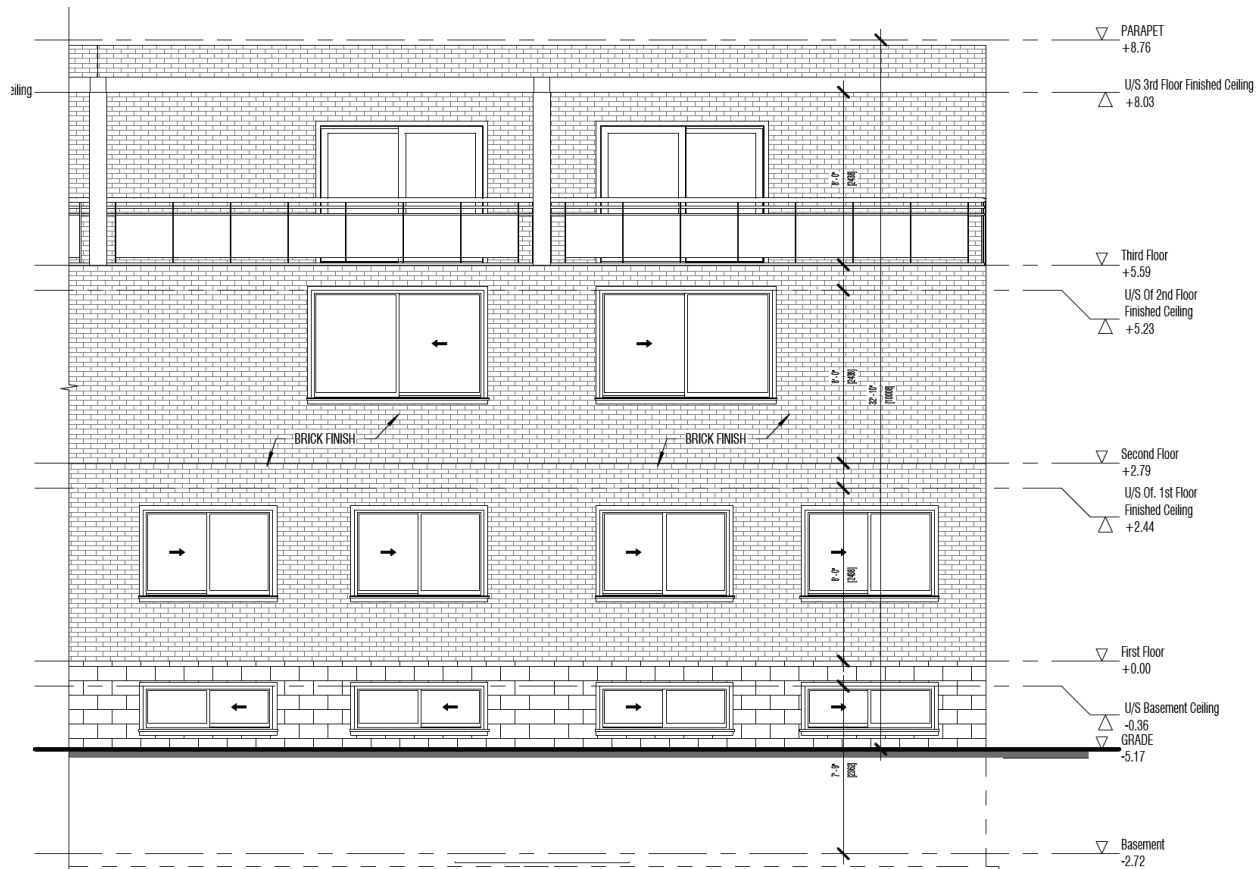


Figure 6 – Block C Rear Elevation

Windows throughout the development are sized and proportioned appropriately with the scale of the development, featured prominently on both the front and rear facades. The siting of buildings to face public streets and/or internal drive aisles where possible provides a visual connection between the unit and the street. The windows have been aligned vertically with a clear rhythm to avoid overcrowding the façade. Different cladding materials such as brick and wood siding finish frame the windows of the development, emphasizing their prominence in the façade.

The use of flat roofs throughout the development establishes a consistent architectural style and a visual connection from each residential block. The roof is clad with wood siding finish on the front façade and brick finish on the side and rear facades, which appropriately complements the exterior cladding materials that are used for a cohesive built form.

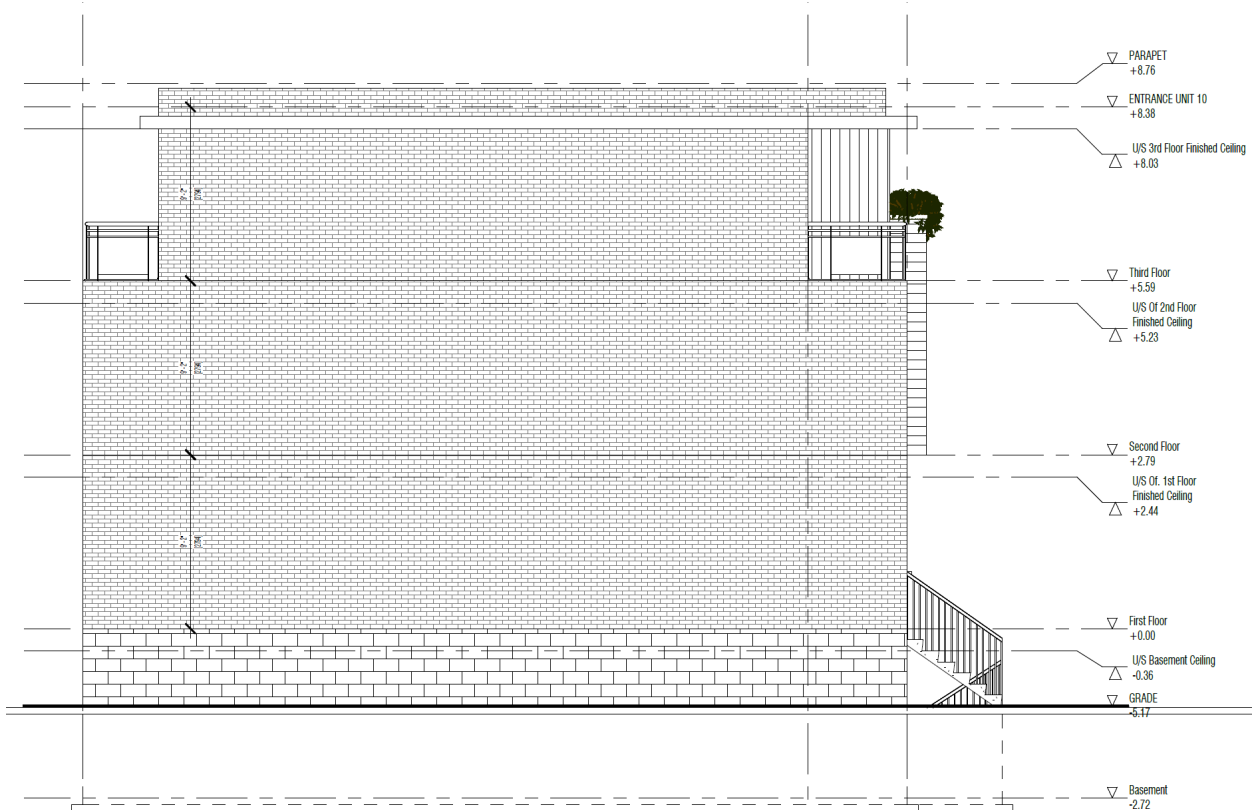


Figure 7 – Block C Side Elevation

In summary, the proposed development presents a building design and architectural style that sensitively responds to a local context comprised of contemporary architectural styles. The proposed development presents a unique architectural expression that respects the low-rise context of the neighbourhood, while fostering design excellence through high-quality built form and careful attention to detail.

SUMMARY

The proposed development has regard for the urban design policies and direction as set out in the NOP, Region’s Model Urban Design Guidelines, and the City’s Official Plan and represents a well-designed development that positively contributes to the surrounding built environment and character of the neighbourhood. The proposed development demonstrates a high standard of design excellence through the building and site design, massing, and setbacks, which effectively address the location and existing context of the area, complements the surrounding properties, adds visual variety to the streetscape, and overall enhances both the public and private realm.

7.0 Proposed Zoning By-law Amendment

The Subject Lands are presently zoned *Residential Low Density, Grouped Multiple Dwellings (R4) Zone* in accordance with Zoning By-law No. 79-200, as amended (see **Appendix D – City of Niagara Falls Zoning By-law 79-200**).

A Zoning By-law Amendment is requested to facilitate the proposed development. The Zoning By-law Amendment proposes rezoning of the Subject Lands from *Residential Low Density, Grouped Multiple Dwellings (R4) Zone* to a site-specific *Residential Low Density, Grouped Multiple Dwellings (R4-XX) Zone*, to address deficiencies related to lot area, front yard depth, rear yard depth, interior side yard, parking requirements, landscaped open space, amenity space, and maneuvering aisle width.

The following comprehensive zoning table reviews the proposed development against the current regulations of the R4 zone.

A draft Zoning By-law Amendment has been prepared and attached in **Appendix E – Draft Zoning By-law**.

7.1 Zoning Compliance Review

Table 9 - Zoning Compliance, Residential Low Density, Grouped Multiple Dwellings (R4) Zone

No.	REGULATION	REQUIRED	PROPOSED	COMPLIANCE
Section 7.9 – Residential Low Density, Grouped Multiple Dwellings (R4)				
7.9.1	Permitted Uses	A stacked townhouse dwelling	Stacked townhouse dwelling	
7.9.2 (a) (ii)	Minimum Lot Area	200 sqm/unit 200 sqm x 42 units = 8,400 sqm	95.85 sqm/unit 95.85 sqm x 42 units = 4,025.68 sqm (after road widening)	No
7.9.2 (b) (i)	Minimum Lot Frontage	30 m	46.33m	Yes
7.9.2 (c) (ii)	Minimum Front Yard Depth for a stacked townhouse dwelling	7.5m plus 15.25 metres from the original centreline of McLeod Road (from Oakwood Drive to Marineland Parkway)	2.79m to wood siding projection 18.05m from the McLeod Road centreline	No
7.9.2 (d) (ii)	Minimum Rear Yard Depth for a stacked townhouse dwelling	10m plus any applicable distance specified in section 4.27.1	6m	No

No.	REGULATION	REQUIRED	PROPOSED	COMPLIANCE
7.9.2 (e)	Minimum Interior Side Yard	One-half the height of the building 5m	Block A: 2.77m (west) 6.25m (east) Block B: 6m (west) 11.4m (east) Block C: 3m (west) 3m (east)	No Yes Yes Yes No No
7.9.2 (f) (ii)	Minimum Exterior Side Yard Width for a stacked townhouse dwelling	7.5m plus any applicable distance specified in section 4.27.1	N/A	N/A
7.9.2 (g)	Maximum Lot Coverage	35%	25.11%	Yes
7.9.2 (h)	Maximum Height of Building or Structure	10m subject to section 4.7	10m	Yes
7.9.2 (j)	Number of Apartment Dwellings on One Lot	Subject to compliance with section 7.9.3, more than one dwelling is permitted on one lot	42 stacked townhouse dwellings	Yes
7.9.2 (k)	Parking and Access Requirements	1.4 space/unit 1.4 space x 42 units = 59 spaces	1.1 space/unit 1.1 space x 42 units = 46 spaces	No
7.9.2 (l)	Accessory Buildings and Accessory Structures	In accordance with Sections 4.13 and 4.14	N/A	N/A
7.9.2 (m)	Minimum Landscaped Open Space Area	45 sqm/unit 45 sqm x 42 units = 1,890 sqm	20.73 sqm/unit 20.73 sqm x 42 units = 870.66 sqm	No
7.9.2 (o)	Minimum Amenity	20 sqm/unit	10.47 sqm/unit	No

No.	REGULATION	REQUIRED	PROPOSED	COMPLIANCE
	Space for a stacked townhouse dwelling	20 sqm x 42 units = 840 sqm	10.47 sqm x 42 units = 439.94 sqm	
Section 4 – General Provisions				
4.14	Yards	<i>Subject to Section 4.27.1, a deck may project into a required front yard a distance of not more than 2.5m</i>	1.29m from dwelling stairs	Yes
4.19.1 (d)	Minimum Perpendicular Parking Stall Width	2.75 m	2.75m	Yes
4.19.1 (e)	Minimum Perpendicular Length of Parking Space	<i>Parking Space Angle more than 60 degrees up to 90 degrees</i> 6m	6m	Yes
4.19.1 (f)	Minimum Perpendicular Width of Maneuvering Aisle	6.3m	6m 3.75m + 3.75m easement over abutting lands for drive aisle	No
4.27	Road Allowance Requirements for McLeod Road from Oakwood Drive to Marineland Parkway	2.15m widening at west limit and 5.2m widening at east limit	2.15m widening at west limit and 5.2m widening at east limit	Yes

7.2 Planning Justification

Minimum Lot Area

Required: 200 sqm/unit for 8,400 sqm

Proposed: 95.85 sqm/unit for 4,025.68 sqm

The proposed minimum lot area is required to facilitate a medium density residential development. The density of the proposed development is permitted per Policy 5.4.1 of

the City's Official Plan and is encouraged along the McLeod Road intensification corridor. The proposed development is well-designed, with appropriate parking, landscaping, and setbacks provided.

Minimum Front Yard Depth for a Stacked Townhouse Dwelling

Required: *7.5m plus 15.25 metres from the original centreline of McLeod Road (from Oakwood Drive to Marineland Parkway)*

Proposed: *2.7m to wood siding projection*

The front façade of Block A features landscaping that frames the front of the development, along with a pedestrian walkway and additional proposed landscaping along the frontage which consists of perennials, ornamental grasses, and deciduous trees. These landscaping elements contribute to an enhanced frontage along McLeod Road, and provide an appropriate transition between the private and public realm. Furthermore, the front yard depth of the proposed development is largely impacted by the required road widening, with 2.15 metres required to be dedicated for 6665 McLeod Road, and 5.2 metres required to be dedicated for 6645 & 6655 McLeod Road. The proposed front yard depth is not anticipated to have any negative impacts on the streetscape. The proposed landscaping and orientation of the development closer to the street contributes to an enhanced street presence along an intensification corridor.

Minimum Rear Yard Depth for a Stacked Townhouse Dwelling

Required: *10m plus any applicable distance specified in section 4.27.1*

Proposed: 6m

The intent of this provision is to ensure that sufficient separation distance is provided between properties to minimize potential conflicts. The proposed rear yard setback is appropriate for the proposed development as it maintains a proper distance between Block C and the adjacent property, and accommodates landscaping measures such as board fencing 1.8 metres in height and a row of three deciduous trees along the rear yard. This presents an appropriate separation distance while providing for effective screening between the properties.

Minimum Interior Side Yard

Required: *One-half the height of the building*

5m

Proposed: *Block A:*

2.77m (west)

Block C:

3m (east and west)

The proposed development provides for various mitigation measures between the Subject Lands and neighbouring properties through landscaping and design. Timber perimeter fencing 1.8-metre in height is proposed along the perimeters of the property, with various landscaping measures such as tree and shrub plantings, and preservation of existing trees along the western property line, that act as effective screening and buffering between adjacent uses. The requested 2.7 metre and 3 metre interior side yard setback are only for the side wall of Block A and Block C, respectively – no windows are proposed on the side facades of the proposed development which minimizes potential overlook issues with neighbouring properties. Where the rear wall of Block B abuts the side lot line, the proposed development exceeds the required interior side yard, which provides a suitable rear yard condition. The proposed interior side yard setbacks are appropriate for the proposed development.

Parking and Access Requirements

Required: 1.4 space/unit

$$1.4 \text{ space} \times 42 \text{ units} = 59 \text{ spaces}$$

Proposed: 1.1 space/unit

$$1.1 \text{ space} \times 42 \text{ units} = 46 \text{ spaces}$$

The proposed development is within walking distance to various local amenities and transit routes that provide both intercity and interregional connections. Furthermore, the proposed parking ratio of 1.1 space per unit is supported by the City's Transportation staff, as noted in the Pre-Consultation Agreement dated July 18, 2024. Given the existing context, the proposed parking reduction is appropriate for the proposed development.

Minimum Landscaped Open Space Area

Required: 45 sqm/unit

$$45 \text{ sqm} \times 42 \text{ units} = 1,890 \text{ sqm}$$

Proposed: 21.1 sqm/unit

$$21.1 \text{ sqm} \times 42 \text{ units} = 886.2 \text{ sqm}$$

The requested reduction is appropriate for the proposed development and can be supported for the following reasons:

- The proposed development does not meet the required 45 sqm/unit minimum landscaped open space requirement in order to accommodate surface parking for the proposed development. This is desirable to ensure adequate parking spaces for the development in a manner that reduces impacts on the streetscape and adjacent land-uses. Although the inclusion of underground parking for the proposal may be desirable from an urban design perspective, doing so would impact the affordability of the dwelling units. The landscaping deficiency is also impacted by the imposed road-widening which removes area from the lands. It is anticipated

that the road widening would provide for greater landscaping along the site frontage.

- The proposed landscaped area will be made up of landscaping and tree plantings, sod areas, and surface walkways that enhance the proposed development, facilitate safe, direct, and efficient pedestrian movement throughout the site and provide a space for residents to interact outdoors. Additionally, although the unit balconies do not contribute to the landscaped area calculation, some dwelling units in the development contain private balconies that provides a similar function in terms of amenity and open space area.
- The proposed landscaped open space will not pose any issues for stormwater management for the development. Stormwater for the development will be handled through LID practices, a new storm pipe flow system, and a combination of underground storm sewer and storm structures. Please refer to the Stormwater Management Report prepared by MXL Engineering & Associates, dated February 3rd, 2025, for more details.
- The site is close to various City parks and recreational spaces such as Prince Charles Park, Wilson Park, Fern Park, and Westfield Park (please refer to **Figure 2 – Community Amenities Map**). Proximity to such amenities provides residents with recreational and leisurely opportunities for walking, socializing, and exercising.

Minimum Amenity Space for a Stacked Townhouse Dwelling

Required: 20 sqm/unit

$$20 \text{ sqm} \times 42 \text{ units} = 840 \text{ sqm}$$

Proposed: 10.45 sqm/unit

$$10.45 \text{ sqm} \times 42 \text{ units} = 439 \text{ sqm}$$

The proposed development will have 439 sqm of amenity space, consisting of landscaped open space, private rear yards for Blocks B and C, and balconies for some units that provide a private space for their use and enjoyment. Furthermore, the proposed development is in proximity to various park spaces as previously mentioned, all of which are within a 10- to 15-minute walk, which provides residents with ease of access to recreational opportunities. The proposed amenity area is also similar to amenity area provided within other nearby stacked townhouse developments. The requested reduction is therefore appropriate for the proposed development and can be supported.

Minimum Perpendicular Width of Maneuvering Aisle

Required: 6.3m

Proposed: 3.75m and 6m

The proposed 6-metre width for the parking drive aisle is sufficient to accommodate maneuvering vehicles and would not impact traffic flow or cause vehicular conflicts. Furthermore, the 3.75 metre width requested would be part of the shared drive aisle with

the neighbouring property, for a full driveway width of 7 metres. The combined drive aisle dimensions exceed the City's requirements. Lastly, the City's Transportation staff have stated that they support this reduction in aisle width as noted in the Pre-Consultation Agreement dated July 18, 2024.

8.0 Summary and Conclusion

The proposed development is located on McLeod Road in proximity to nearby commercial and recreational amenities. The location of the proposal is well serviced by existing transit and will contribute to the intensification of the McLeod Road Corridor. The proposed 42 stacked townhouse dwelling units make efficient use of the Subject Lands and will increase housing options in the City of Niagara Falls.

It is our opinion that the proposed Zoning By-law Amendment and Consent represent good land use planning, are in the public interest, and should be approved for the following reasons:

1. The Applications are consistent with the Provincial Planning Statement (2024) and in conformity with the Niagara Official Plan and the City of Niagara Falls Official Plan.
2. The Applications will provide an opportunity for residential intensification along McLeod Road in an area in proximity to commercial, institutional, and recreational uses, along with access to public transit and other active transportation choices.
3. The Applications represent a compact and transit-supportive built form that will make efficient use of existing municipal services and facilities and support the achievement of complete communities.
4. The Applications will create new housing in the City of Niagara Falls with a mix of unit types and sizes, contributing to more diverse mix of housing options, including 28 housing units affordable to moderate income households.
5. The Applications are informed by good urban design principles and practices that:
 - a. ensure a compatible design with the existing neighbourhood that is appropriately scaled and sited;
 - b. promote a safe, walkable, and pedestrian-oriented development through direct pedestrian walkways and landscaping throughout the development; and
 - c. contribute to a vibrant and pleasant streetscape experience along McLeod Road through enhanced landscaping and a high-quality of architectural design.
6. The proposed shared driveway configuration makes an efficient use of land by:
 - a. reducing impervious surfaces;
 - b. limiting access to and minimizing traffic impacts on McLeod Road (a Regional road); and
 - c. integrating appropriately with the neighbouring property through a comprehensive design.

Report prepared by:



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Intermediate Planner
NPG Planning Solutions Inc.

Report reviewed and approved by:



Aaron Butler, MCIP, RPP
Principal Planner, Niagara
NPG Planning Solutions Inc.

9.0 Appendices

Appendix A – Site Plan

Appendix B – Niagara Official Plan Schedules

Appendix C – City of Niagara Falls Official Plan Schedules

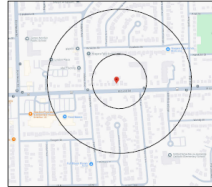
Appendix D – City of Niagara Falls Zoning By-law 79-200

Appendix E – Draft Zoning By-law Amendment

Appendix A – Site Plan

6645, 6655, 6665 McLeod Road – Planning Justification Report & Urban Design Brief

MUNICIPALITY & PROJECT INFORMATION	
PROJECT TYPE & DESCRIPTION	6655 McLeod Rd
MUNICIPALITY	NAGARA
ZONING DESIGNATION	R4

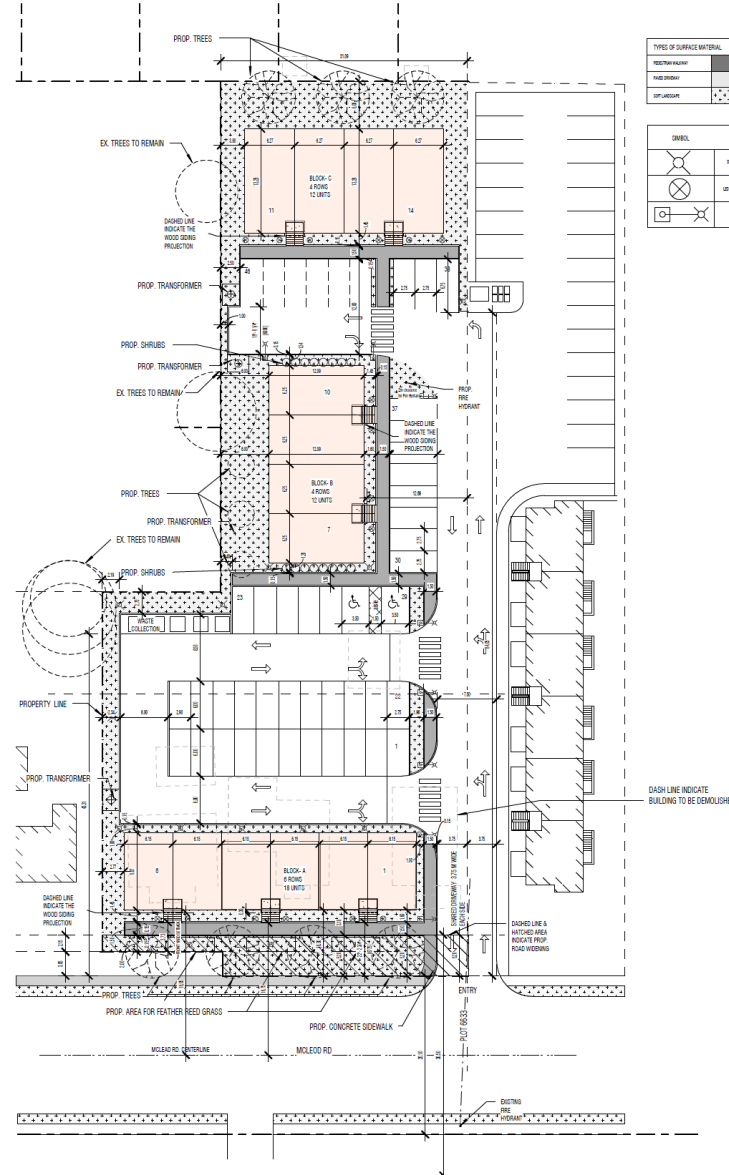


LOT SIZE	EXISTING
- NORTH (FRONT)	102.00 x 331.00 m
- SOUTH (FRONT)	217.46 x 166.26 m
- WEST	366.88 x 112.62 m
- EAST	366.40 x 112.50 m
TOTAL LOT AREA	
4542.49 sq. ft. / 420.12 sq. m.	
TOTAL LOT AREA w/o road widening area	
4332.46 sq. ft. / 402.66 sq. m.	

R4 ZONE REGULATIONS

Provision	Requirement	Proposal
(a) Minimum lot area		
(i) for an apartment dwelling or stacked townhouse dwelling	200 square metres (2,152.8 sq. ft.) for each dwelling unit	95.65 sqm (1031.72 sq. ft.) for each dwelling unit 42 Units x 95.65 sqm = 4025.68 sq. m. (w/o road widening area)
(b) Minimum lot frontage		
(i) for a townhouse dwelling or an apartment dwelling or stacked townhouse dwelling containing more than four dwelling units	30 metres (98.4 ft.)	46.33 m (152 ft.)
(c) Minimum front yard setback		
(i) for an apartment dwelling or stacked townhouse dwelling	7.5 metres (24.61 ft.) (plus any applicable distance specified in section 4.27.1)	3.73 m (12.20 ft.)
(d) Minimum rear yard setback		
(i) for an apartment dwelling or stacked townhouse dwelling	10 metres (32.81 ft.) (plus any applicable distance specified in section 4.27.1)	5.77 m (18.96 ft.)
(e) Minimum interior side yard setback	one-half the height of the building (10/2 = 5 m)	2.77 m (9.08 ft.)
(f) Minimum exterior side yard width		
(i) for an apartment dwelling or stacked townhouse dwelling	7.5 metres plus any applicable distance 2x6 ft. specified in section 4.27.1	N/A
(g) Maximum lot coverage	35%	25.11%
(h) Maximum height of building or structure	10 metres (32.81 ft.) subject to section 4.7	Block A - 10 m (32.81 ft.) Block B - 10 m (32.81 ft.) Block C - 10 m (32.81 ft.)
(i) Devised by By-law No. 2011-136		
(j) Number of dwellings on one lot	subject to compliance with section 7.9.3, more than one dwelling is permitted on one lot	3
(k) Parking and access requirements	In accordance with section 4.19.1 42 units x 1.4 = 58.8 parking spaces	46 parking spaces
(l) Accessory buildings and accessory structures	In accordance with sections 4.13 and 4.14	
(m) Minimum landscaped open space	45 square metres (484.4 sq. ft.) for each dwelling unit	20.73 sqm (223.14 sq. ft.) for each dwelling unit
(n) Minimum privacy yard depth for each townhouse dwelling unit, as measured from the exterior rear wall of every dwelling unit (2009-148)	7.5 metres	N/A
(o) Minimum amenity space for an apartment dwelling unit	Subject to compliance with section 4.44, Min. 20 sqm per dwelling unit (42 units)	439.94 sqm
(p) Total soft landscape area	N/A	871.04 sq. m. (21,43.5% of the LOT AREA w/o road widening)

Net Floor Area		Gross Floor Area (GFA) - Block A		Gross Floor Area (GFA) - Block B		Gross Floor Area (GFA) - Block C	
BLOCK/UNIT	Area	BLOCK A - UNIT 1	Area	BLOCK B - UNIT 1	Area	BLOCK C - UNIT 1	Area
BLOCK A - UNIT 1-18	556 m ²	BLOCK A - UNIT 1	635 m ²	BLOCK B - UNIT 1	739 m ²	BLOCK C - UNIT 1	874 m ²
BLOCK A - UNIT 2-17	483 m ²	BLOCK A - UNIT 2	562 m ²	BLOCK B - UNIT 2	741 m ²	BLOCK C - UNIT 2	807 m ²
BLOCK A - UNIT 3-16	461 m ²	BLOCK A - UNIT 3	709 m ²	BLOCK B - UNIT 3	1929 m ²	BLOCK C - UNIT 3	2291 m ²
BLOCK A - UNIT 4-15	461 m ²	BLOCK A - UNIT 4	709 m ²	BLOCK B - UNIT 4	1529 m ²	BLOCK C - UNIT 4	2291 m ²
BLOCK A - UNIT 5-14	500 m ²	BLOCK A - UNIT 5	562 m ²	BLOCK B - UNIT 5	741 m ²	BLOCK C - UNIT 5	807 m ²
BLOCK A - UNIT 6-13	533 m ²	BLOCK A - UNIT 6	635 m ²	BLOCK B - UNIT 6	739 m ²	BLOCK C - UNIT 6	874 m ²
BLOCK A - UNIT 7-12	533 m ²	BLOCK A - UNIT 7	635 m ²	BLOCK B - UNIT 7	739 m ²	BLOCK C - UNIT 7	874 m ²
BLOCK A - UNIT 8-11	641 m ²	BLOCK A - UNIT 8	562 m ²	BLOCK B - UNIT 8	2219 m ²	BLOCK C - UNIT 8	2445 m ²
BLOCK B - UNIT 2-11	913 m ²	BLOCK A - UNIT 9	1234 m ²	BLOCK B - UNIT 9	1529 m ²	BLOCK C - UNIT 9	1708 m ²
BLOCK B - UNIT 3-10	1154 m ²	BLOCK A - UNIT 10	1234 m ²	BLOCK B - UNIT 10	52 m ²	BLOCK C - UNIT 10	67 m ²
BLOCK B - UNIT 4-9	1166 m ²	BLOCK A - UNIT 11	562 m ²	BLOCK B - UNIT 11	741 m ²	BLOCK C - UNIT 11	807 m ²
BLOCK B - UNIT 5-8	647 m ²	BLOCK A - UNIT 12	635 m ²	BLOCK B - UNIT 12	739 m ²	BLOCK C - UNIT 12	874 m ²
BLOCK B - UNIT 6-7	676 m ²	BLOCK A - UNIT 13	635 m ²	BLOCK B - UNIT 13	63 m ²		
BLOCK C - UNIT 1-12	715 m ²	BLOCK A - UNIT 14	562 m ²				
BLOCK C - UNIT 2-11	683 m ²	BLOCK A - UNIT 15	1234 m ²				
BLOCK C - UNIT 3-10	1321 m ²	BLOCK A - UNIT 16	1234 m ²				
BLOCK C - UNIT 4-9	1384 m ²	BLOCK A - UNIT 17	562 m ²				
BLOCK C - UNIT 5-8	706 m ²	BLOCK A - UNIT 18	635 m ²				
BLOCK C - UNIT 6-7	783 m ²						
Grand Total	14423 m ²						



TYPE OF SURFACE MATERIAL	
ASPHALT	[Symbol]
CONCRETE	[Symbol]
GRAVEL	[Symbol]

SYMBOL	DESCRIPTION	DIMENSIONS
[Symbol]	REINFORCING BARS	1214 x 1214
[Symbol]	CONCRETE FINISH LINES	1214 x 1214
[Symbol]	PROP. CURB	1214 x 1214

MXL ENGINEERING & ASSOCIATES, INC.
1649 ST. Clair Avenue W. TORONTO ON
M6R1K7
email: permis@mxleng.ca
website: www.mxleng.ca
Phone: 416-665-4033

NO.	DESCRIPTION OF ISSUE	DATE
1	ISSUED FOR CLIENT APPROVAL	10/10/2024
2		
3		
4		
5		
6		
7		

DRAWN BY
RP

APPROVED BY
IK

PROJECT
6645, 6655 & 6665
McLeod Road

TITLE
Site Plan

SCALE
As indicated

DATE
Nov. 13 2024

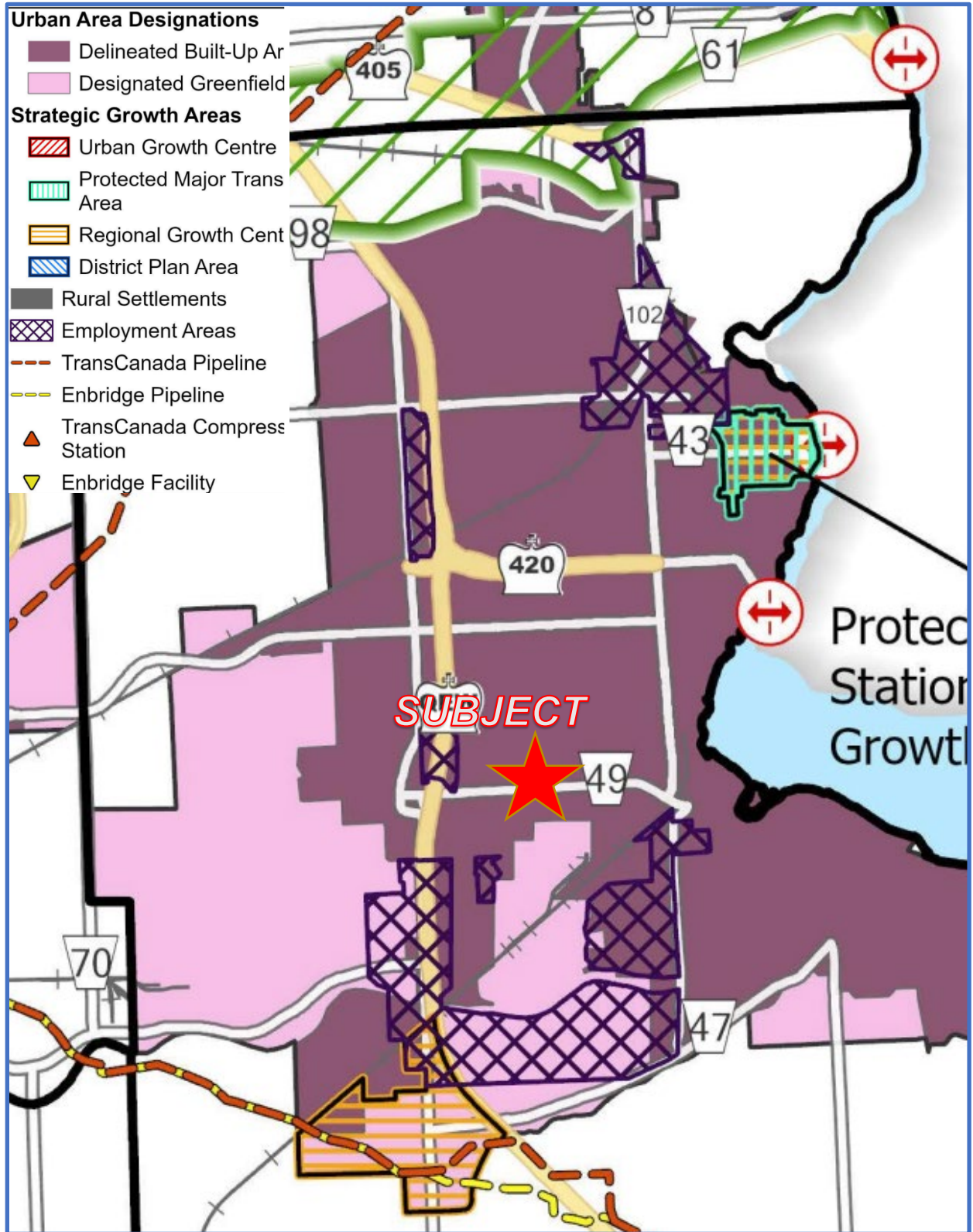
JOB No.
227-2947

DRAWING No.
A01.01

1 SITE PLAN
SCALE: 1" = 30'-0"

Appendix B – Niagara Official Plan Schedules

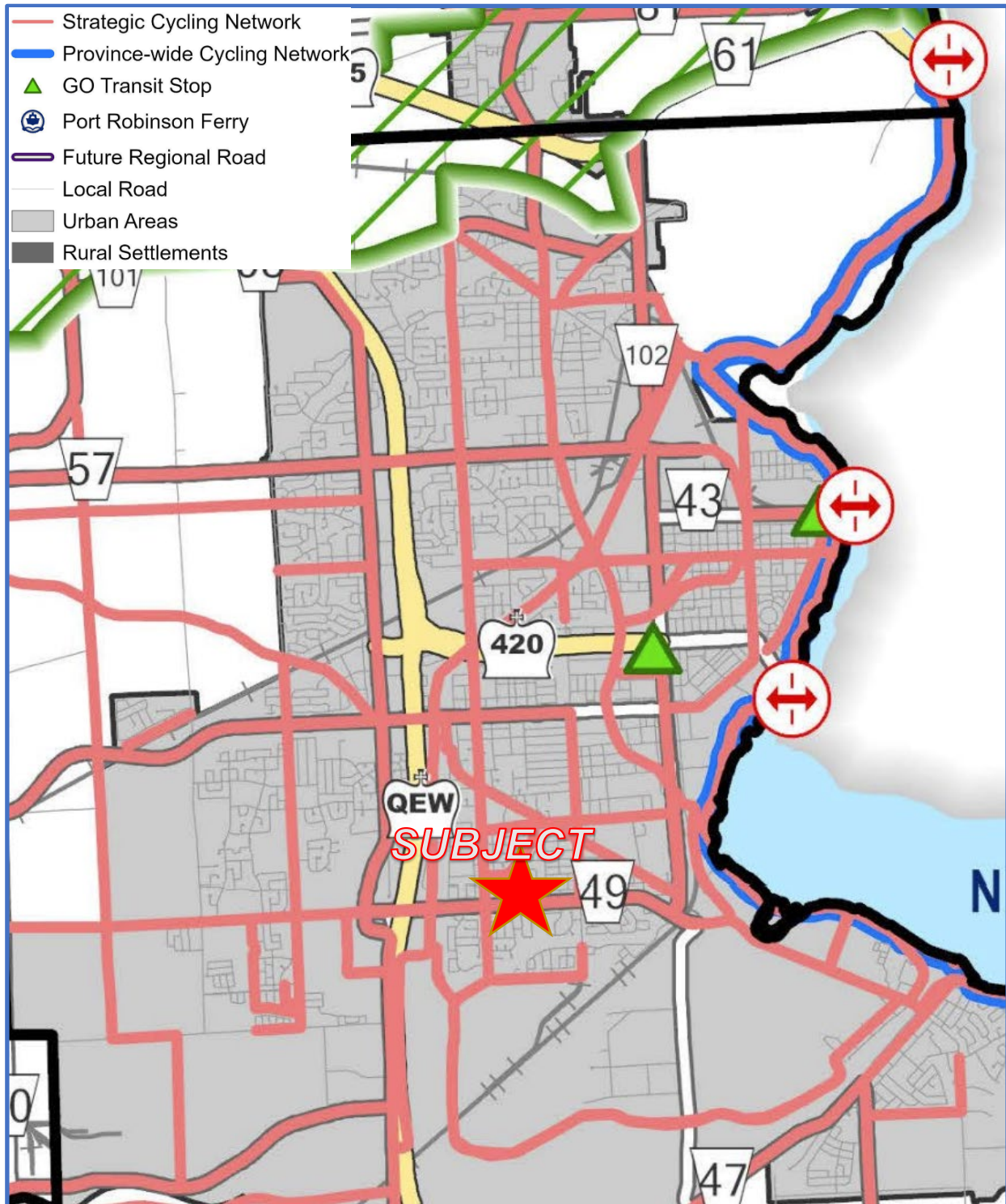
SCHEDULE B – REGIONAL STRUCTURE, *Delineated Built-up Area*



SCHEDULE J1 – TRANSPORTATION INFRASTRUCTURE, *On Regional Road 49*

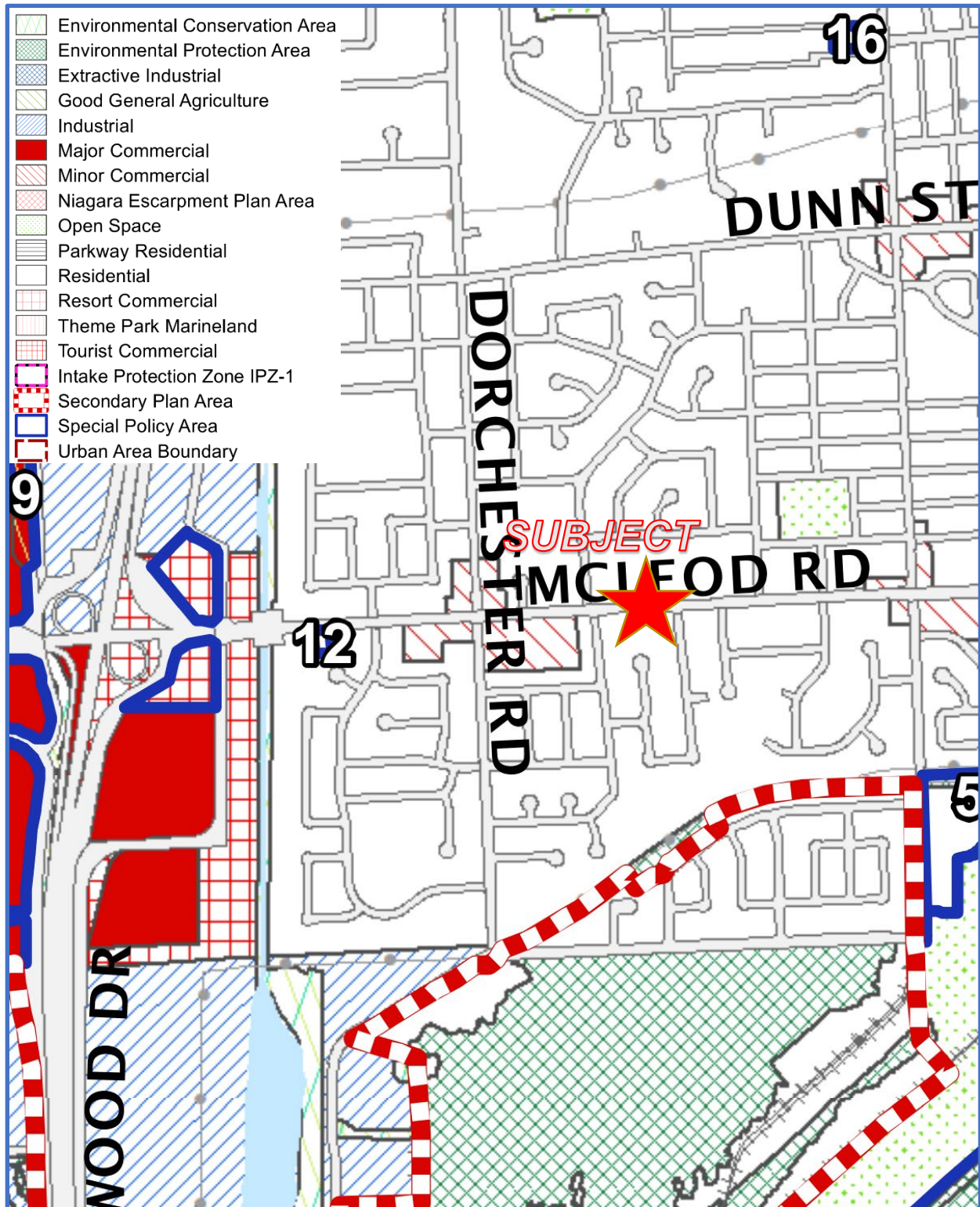


SCHEDULE J2 – STRATEGIC CYCLING NETWORK, *On Strategic Cycling Network*



Appendix C – City of Niagara Falls Official Plan Schedules

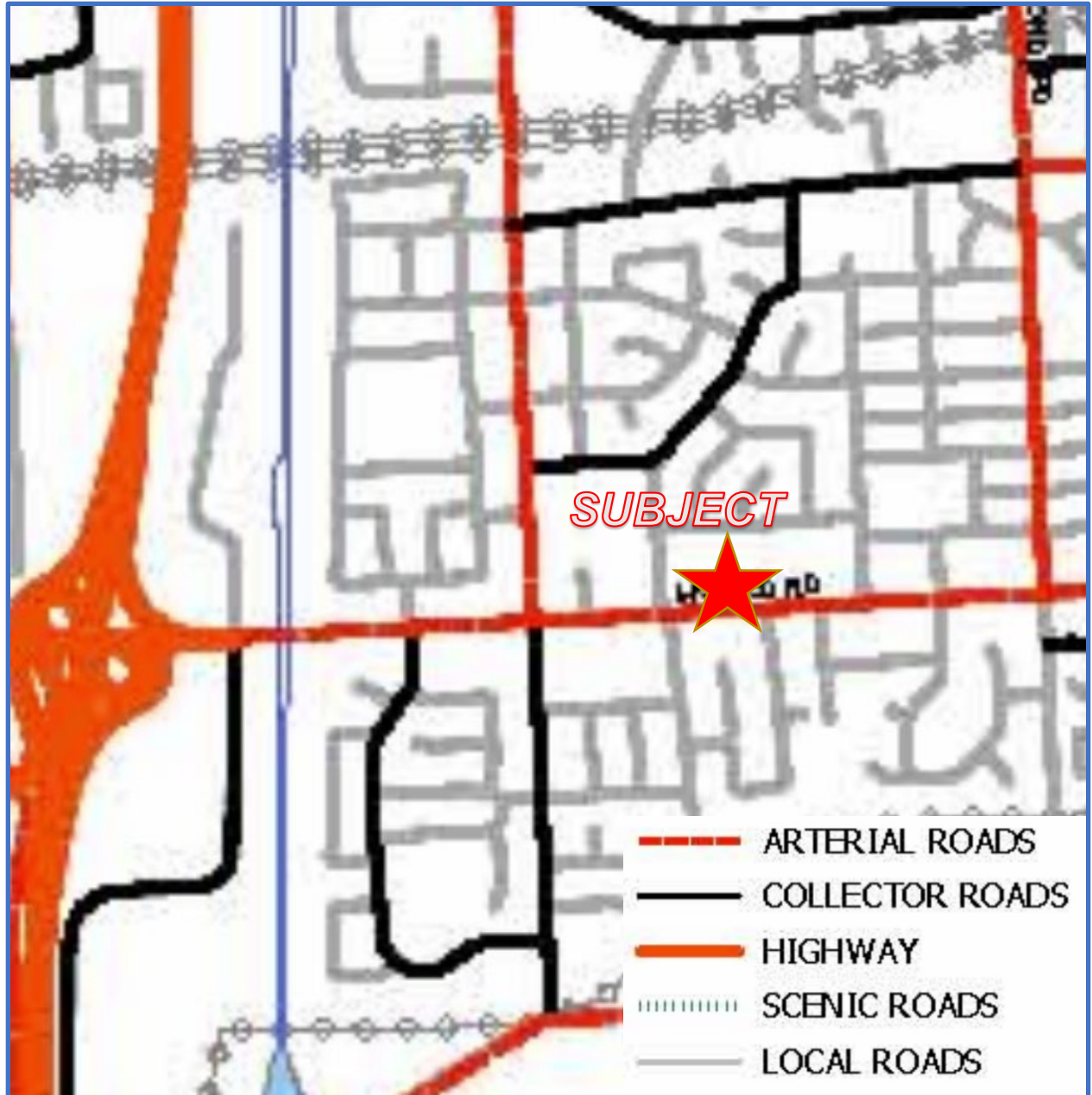
SCHEDULE A – FUTURE LAND USE, *Residential*



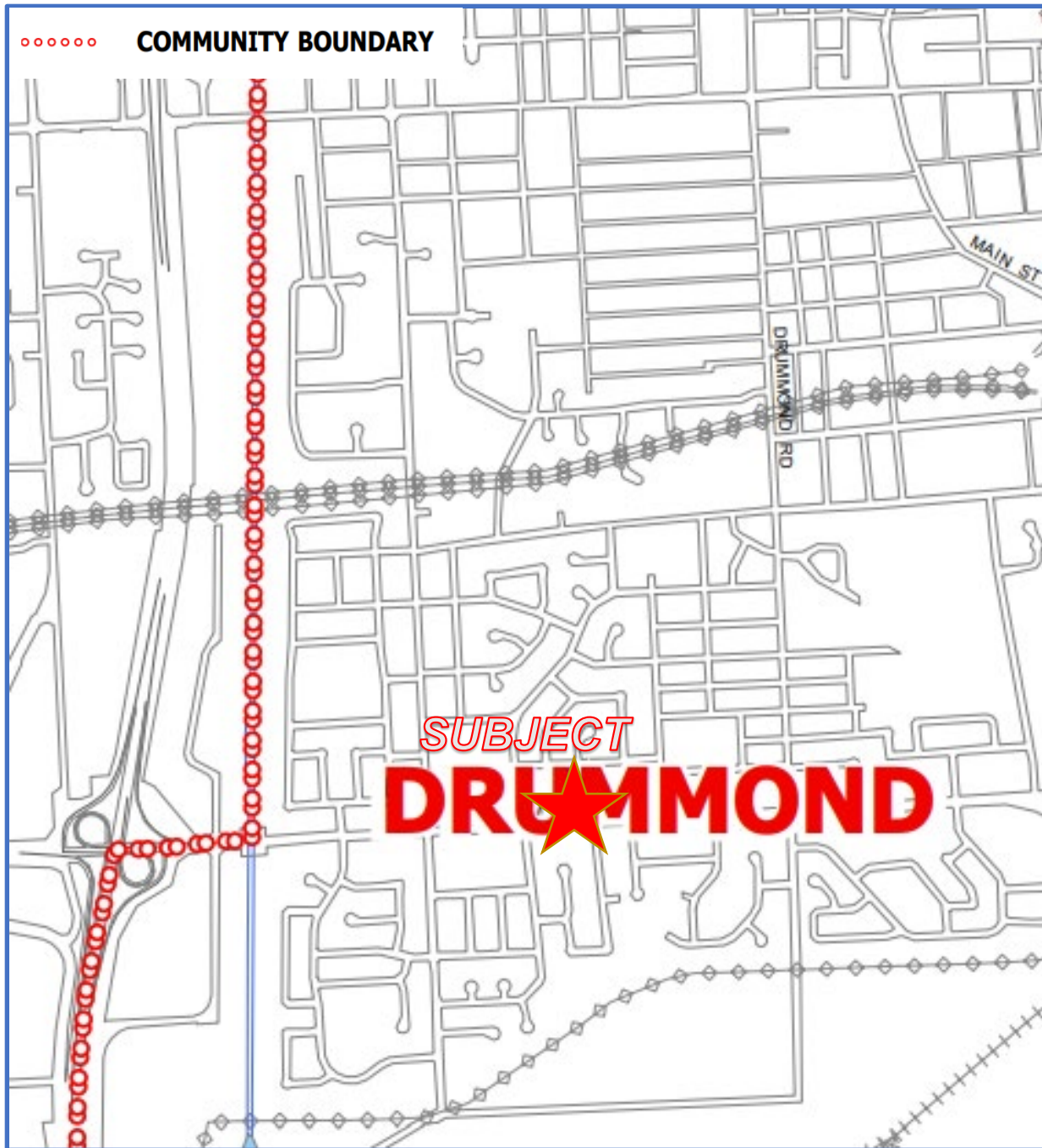
SCHEDULE A2 – URBAN STRUCTURE PLAN, **McLeod Road Corridor & Built Up Area**



SCHEDULE C – MAJOR ROADS PLAN, *Arterial Road: McLeod Road*

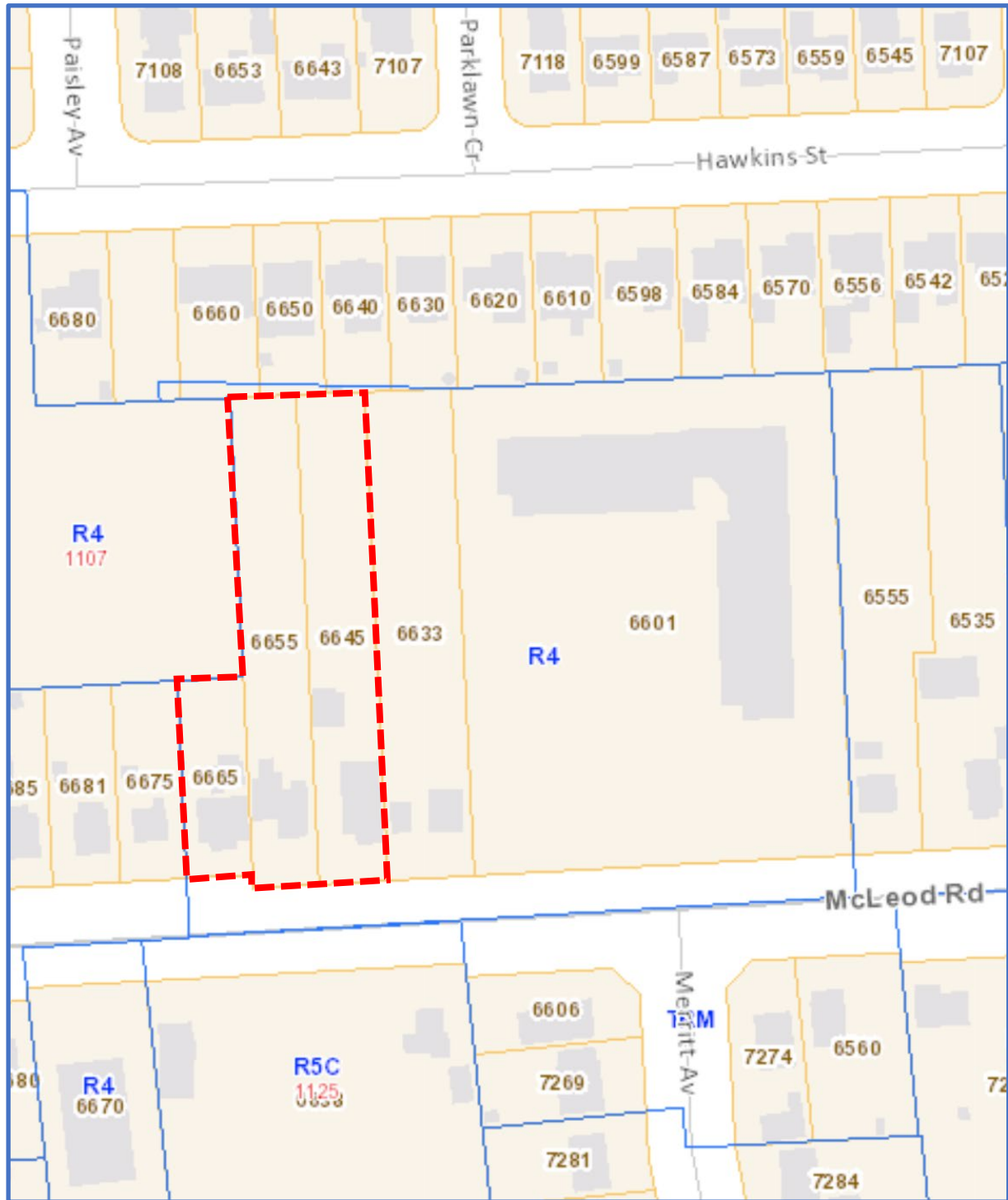


SCHEDULE D – COMMUNITY PLANNING DISTRICT, **Community of Drummond**



Appendix D – City of Niagara Falls Zoning By-law 79-200

ZONING BY-LAW 79-200, **Residential Low Density Grouped Multiple Dwellings Zone (R4 Zone)**



Appendix E – Draft Zoning By-law Amendment

CITY OF NIAGARA FALLS BY-LAW NO. 2025-XX

A BY-LAW TO AMEND BY-LAW NO. 79-200, TO REZONE THE LANDS TO RESIDENTIAL LOW DENSITY, GROUPLD MULTIPLE DWELLINGS (R4-XX) SITE SPECIFIC ZONE (AM-2025-XX).

THE COUNCIL OF THE CORPORATION OF THE CITY OF NIAGARA FALLS ENACTS AS FOLLOWS:

1. The Lands that are the subject of and affected by the provisions of this by-law as described in Schedule 1 of this by-law and shall be referred to in this by-law as the “Lands”. Schedule 1 is a part of this by-law.
2. The purpose of this by-law is to change the zoning of the Lands identified in Schedule 1 from “Residential Low Density, Grouped Multiple Dwellings (R4)” to “Residential Low Density, Grouped Multiple Dwellings (R4-XX) with Site-Specific conditions”.
3. The purpose of this by-law is to amend the provisions of By-law 79-200, to permit the use of the Lands in a manner that would otherwise be prohibited by that by-law. In the case of any conflict between a specific provision of this by-law and any existing provision of By-law No. 79-200, the provisions of this by-law are to prevail.
4. Notwithstanding any provision of By-law No. 79-200 to the contrary, the following uses and regulations shall be the permitted uses and regulations governing the permitted uses on and of the Lands.
5. The permitted uses shall be uses permitted in the R4 Zone.
6. The regulations governing the permitted uses shall be:
 - (a) Minimum Lot Area 95 m² per unit
 - (b) Minimum Front Yard Depth for a Stacked Townhouse Dwelling 2.7m
 - (c) Minimum Rear Yard Depth for a Stacked Townhouse Dwelling 6 metres
 - (d) Minimum Interior Side Yard Block A
2.77 metres (west)
Block C
3 metres (east and west)

- | | | |
|-----|--|---|
| (e) | Parking and Access Requirements | 1.1 space per unit |
| (f) | Minimum Landscaped Open Space Area | 21.1 m ² per unit |
| (g) | Minimum Amenity Space for a Stacked Townhouse Dwelling | 10.45 m ² per unit |
| (h) | Minimum Perpendicular Width of Maneuvering Aisle | 3.75 metres for shared driveway
6 metres for parking aisle |

7. All other applicable regulations set out in By-law No.79-200 shall continue to apply to govern the permitted uses of the Lands, with all necessary changes in detail.
8. No person shall use the Lands in a manner that is contrary to the regulations.
9. The regulations of this By-law shall be shown on Sheet C5 of Schedule “A” of By-law No.79-200 by modifying the lands from R4 to R4-XX.
10. Section 19 of By-law No.79-200 is amended and adding thereto:

19.1.XXXX Refer to By-law No. 2025-XX

Read a first, second and third time; passed, signed and sealed in open Council this _____ DAY OF _____, 2025.

CITY CLERK

JAMES M. DIODATI, MAYOR

