



Urban Design Brief

3151 Montrose Road, Niagara Falls, Ontario



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1 INTRODUCTION

A decorative graphic consisting of seven dark blue squares arranged in a grid-like pattern. The squares are arranged in three rows: the top row has one square on the right; the middle row has two squares; and the bottom row has three squares. The squares are positioned to the right of the word 'INTRODUCTION'.



1.1. INTENT

The Biglieri Group Ltd. (“TBG”) has been retained by 1000441695 Ontario Inc. to prepare an Urban Design Brief in support of the redevelopment of the lands municipally identified as 3151 Montrose Road as well as vacant properties to the north and west in the City of Niagara Falls (“The Subject Site”). The Subject Site is legally described as 59R-13324 PT 02, 03, 05, & 06 PT LOT 46, Stamford. Applications for a Zoning By-law Amendment and future Draft Plan of Condominium are being submitted in support of a nine (9) townhouses, two (2) single-detached dwelling development. This Urban Design Brief builds upon the Planning Rationale Report and demonstrates how the proposed development meets design guidelines and principles set out by Niagara Region and the City of Niagara Falls. The goal of this report is to ensure that the proposed development fosters high quality and sustainable urban design.

1.2. DOCUMENT STRUCTURE

This document is organized into three sections:

- Section 1 - The Policy Context and Site Analysis (Policies, location and community context);
- Section 2 - Vision and Design Principles;
- Section 3 - The Design Concept that is responsive to the policy context and site analysis

2

POLICY CONTEXT & SITE ANALYSIS



2.1. Study Area Description

2.1.1 Site Location

The Subject Site has approximately 76.19 metres of frontage onto Montrose Road. The Subject Site is 0.56 hectares in size and is currently occupied by a single-detached dwelling as well as vacant land to the north and west of the dwelling. As a result of this development, the existing dwelling will be retained. The proposed development consists of nine (9) townhouses and additional two (2) single-detached dwellings, for a total of twelve (12) dwellings on a private condominium road. The proposal will see a total of twenty-nine (29) parking spaces. Five (5) of these spaces will be for visitors. These spaces will be located at grade in between blocks two (2) and four (4). Additional information on parking spaces, and access to the development will be provided at the Site Plan Application stage.

The Subject Topography is relatively flat. The Subject Site gradually slopes down towards the north east corner of the lot. The topography elevation ranges from approximately 194 metres above sea level (ASL) along Montrose Road to approximately 195.7 metres ASL towards the south west corner of the Subject Site.

Examination of the Subject Site larger context reveals the factors that will influence the proposed townhouse units and shape the existing built form:

- The Subject Site fronts onto a Regional Road (Montrose Road) and is located in an established residential neighbourhood.
- The Subject Site is an excellent candidate for residential intensification being adjacent to an Regional Road and within close proximity to the QEW, and a nearby power centre south of the Subject Site.
- Townhouses provide a more compact built form and a more efficient use of land.

2.1.2 Site Context

The Subject Site fronts onto Montrose Road and is 150 metres west of the QEW, with access provided to the south from Thorold Stone Road or Mountain Road to the north.

NORTH

Montrose Road contains of single-detached dwellings. There is a church located just north of the Subject Site, as well as Beach Volleyball courts and the Club Italia event centre. A Hydro One transmission corridor runs directly north of the property.

Lands to the north have been included in the urban boundary and will be developed for residential purposes in the future.

SOUTH

South of the Subject Site are single-detached dwellings along Montrose Road. There is also a low-rise residential building located at 3364 Montrose Road. Mt. Carmel Centre is located at the corner of Montrose Road and Thorold Stone Road. There are a multitude of restaurants, automotive services, and retail stores, as well as a grocery store located in the plaza.

EAST

A cluster of townhomes are located directly east of the Subject Site at 3232 Montrose Road. Further east is the QEW followed by additional single-detached dwellings and the LB Pearson Park. Downtown Niagara Falls is located approximately 4.3 kilometers to the east.

A draft plan of condominium is also proposed for the vacant lands opposite the site

WEST

Single-detached dwellings populate the neighbourhood west of the Subject Site. There are two large parks located south west of 3151 Montrose Road (Shriner's Woodlot Park and Mount Carmel Park).

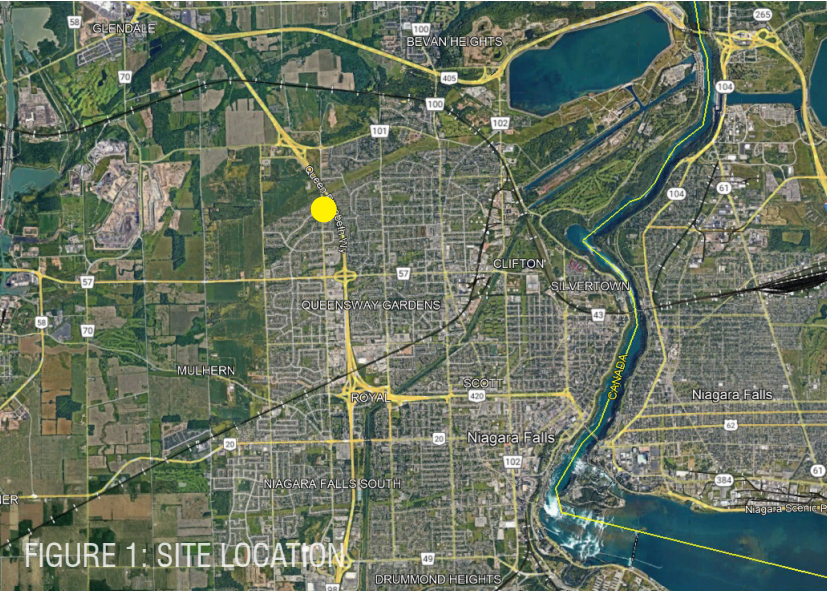


FIGURE 1: SITE LOCATION



FIGURE 2: SITE CONTEXT



NORTH



WEST



Subject Site



EAST



SOUTH

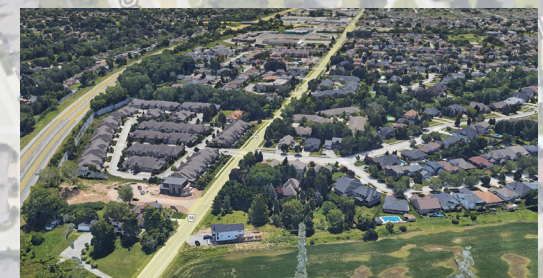


FIGURE 3: SURROUNDING CONTEXT

2.2. Study Area Inventory

PUBLIC SERVICE FACILITIES

The Subject Site is located within proximity to many public service facilities including schools, public transit, hospitals, sports facilities, and parks. Many of these facilities would be easily accessible to future residents of the proposed development via active transportation modes such as walking, biking, or by public transit. The following is a list of nearby public services and facilities:

- The Subject Site is within the jurisdiction of the District School Board of Niagara (DSBN) and the Niagara Catholic District School Board (NCDSB^{spelling}gdgfd). The Subject Site is approximately 1.0 km from the Mary Ward Catholic Elementary School, 1.2 km from Prince Philip Public School, 1.3 km from Orchard Park public school and approximately 1.6 km from Saint Paul Catholic Public School.
- Valour Park is situated approximately 750 m south east of the Subject Site
- The Mount Carmel commercial centre is approximately 900 m south of the Subject Site and includes a variety grocery stores and other commercial uses.
- The Stamford Centre Police Station is approximately 2.2 km south east of the Subject Site. Niagara Falls Station 3 is the closest fire station approximately 1.2 km east of the Subject Site.

TRANSPORTATION

The Subject Site has approximately 76.19 metres of frontage on Montrose Road, which is north/south Regional Road. Queen Elizabeth Way (QEW) is a provincial highway located ~150 meters away from the Subject Site.

The Subject Site is well serviced by the Regional public transportation network. A bus stop is located ~600 meters away from the Subject Site. The Subject Site is serviced by the 105, 209 and 213 routes. These routes provide residents with access to areas south of the Subject Site and throughout the City.

ACTIVE TRANSPORTATION NETWORK

Partial sidewalks are provided on Montrose Road. The western portion of Montrose Road does not have a continuous sidewalk, while the eastern portion close to the Subject Site is not serviced with sidewalks. Matthews Drive and Bishop Avenue to the southwest both have sidewalks to provide for active transportation.

Niagara Region's Transportation Master Plan identifies Montrose Road as an existing cycling facility. The cycling network identified in the Master Plan provides connectivity to the surrounding areas within the Region, such as Niagara-on-the-Lake.

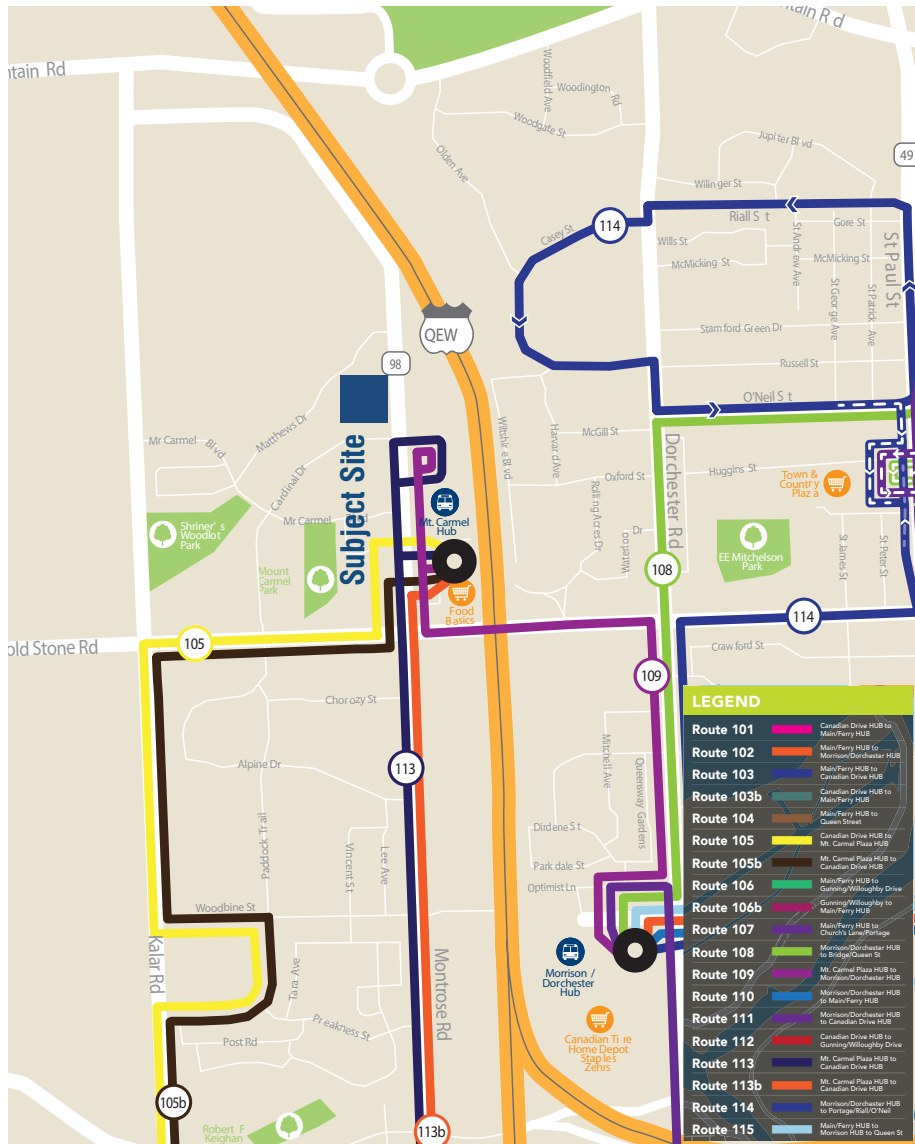


FIGURE 4: NIAGARA REGION TRANSIT MAP

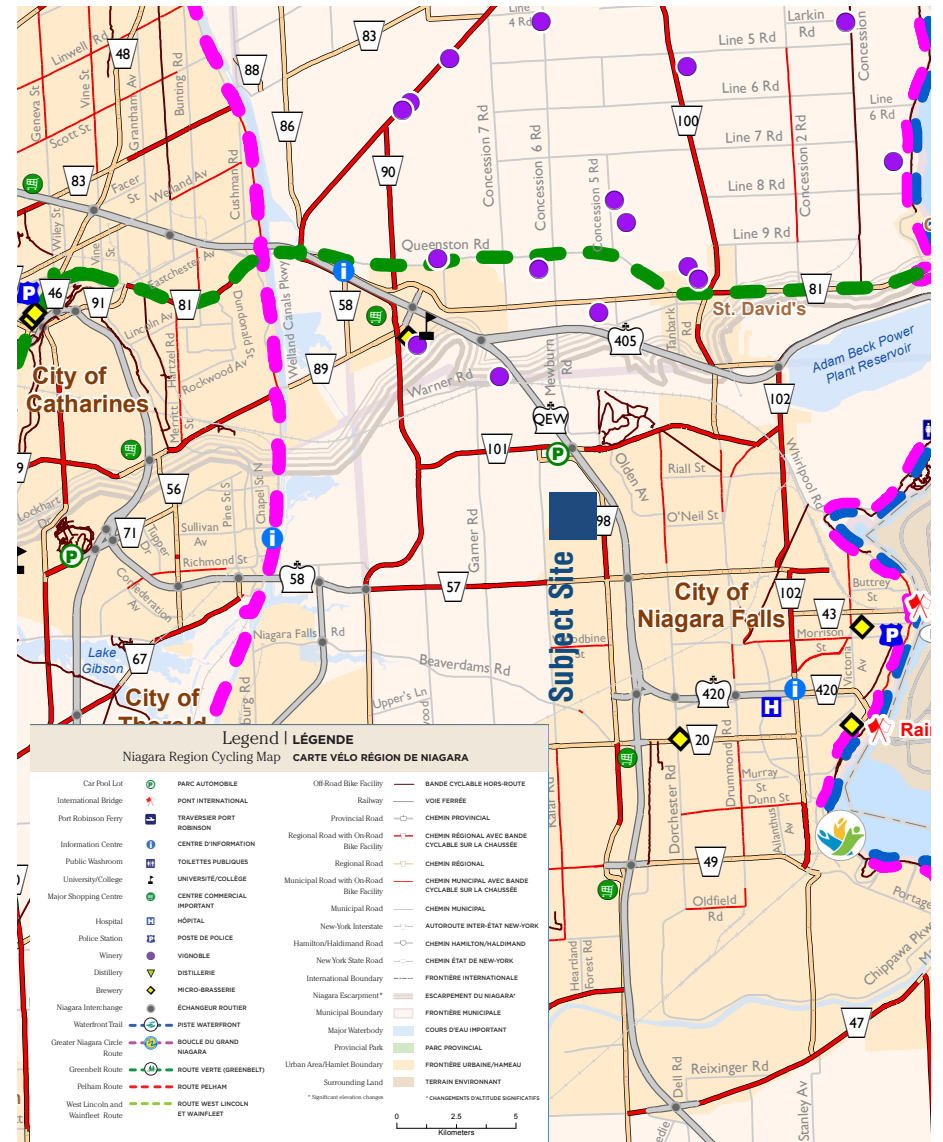
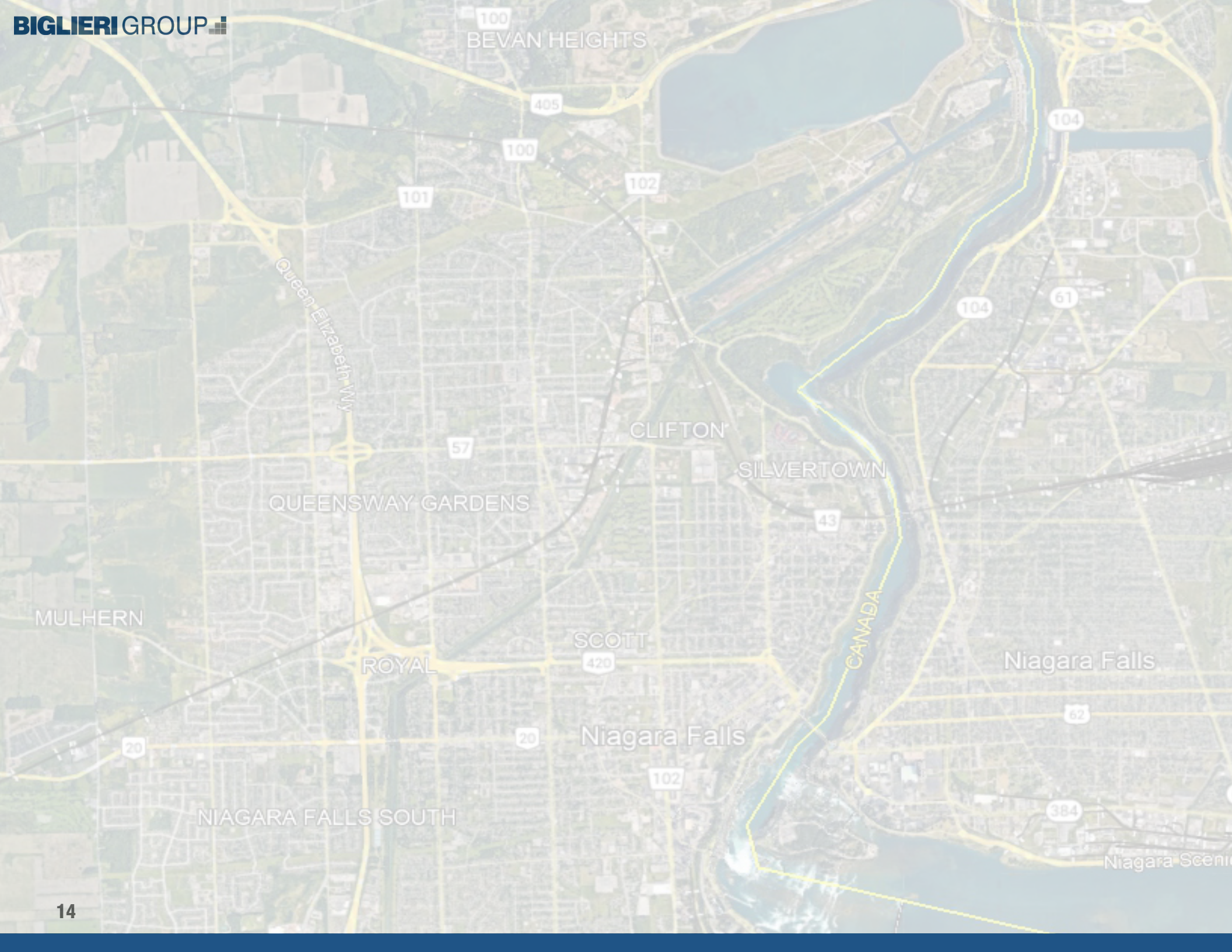


FIGURE 5: NIAGARA REGION CYCLING MAP



100 BEVAN HEIGHTS

405

100

101

102

104

Queen Elizabeth Way

57

CLIFTON

SILVERTOWN

104

61

QUEENSWAY GARDENS

43

MULHERN

SCOTT

420

ROYAL

CANADA

Niagara Falls

20

20

Niagara Falls

62

NIAGARA FALLS SOUTH

102

384

Niagara Scenic

2.2. Planning Policy Enforcement

This Urban Design and Sustainability Brief includes a summary of the planning and policy framework and documents at provincial, regional, and municipal levels, that guide and regulate planning decisions in the **Town of Bowmanville**. Each of the below sections outlines the applicable policies from the different documents and how the proposed development addresses the different goals, directives, and standards.

The proposed development is consistent with the Provincial Policy Statement (2020) and is conforms to the following policies:

- A Place to Grow: Growth Plan for the Greater Golden Horseshoe (2020);
- The Niagara Regional Official Plan (2022 Provincial Approved);
- The City of Niagara Falls Official Plan (Office Consolidation, 2019);
- The Niagara Region Model Urban Design Guidelines.

2.2.1 Provincial Policy Statement (2020) (“PPS”)

Policy

The PPS guides the growth and development within urban and rural settlement areas across Ontario. The policy document directs for efficient land use patterns that support current and future population needs across the province. Policy 1.1.1 of the PPS states that healthy, livable and safe communities are sustained by promoting the integration of land use planning, growth, transit-supportive development, and infrastructure planning that minimizes land consumption and servicing costs; and by promoting development and land use patterns that conserve biodiversity.

In section 1.1.1 of the PPS encourages efficient land use and development patterns to support healthy, livable and safe communities by promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term. An appropriate and market-based range and mix of residential types is also required.

Section 1.1.3.3 of the PPS directs opportunities for transit supportive development that accommodate a significant supply and range of housing options through intensification and redevelopment be promoted where appropriate.

Response

The proposed residential development is consistent with the PPS’s intensifications, mix and uses policies.

The proposed development will:

- Intensify lands within the municipal Built-up Area of Niagara Falls;
- Diversify the housing supply mix and range of residential typologies within Niagara Falls;
- Increase housing supply and housing choice availability in the area;
- The proposed development will make use of existing municipal infrastructure and transit.

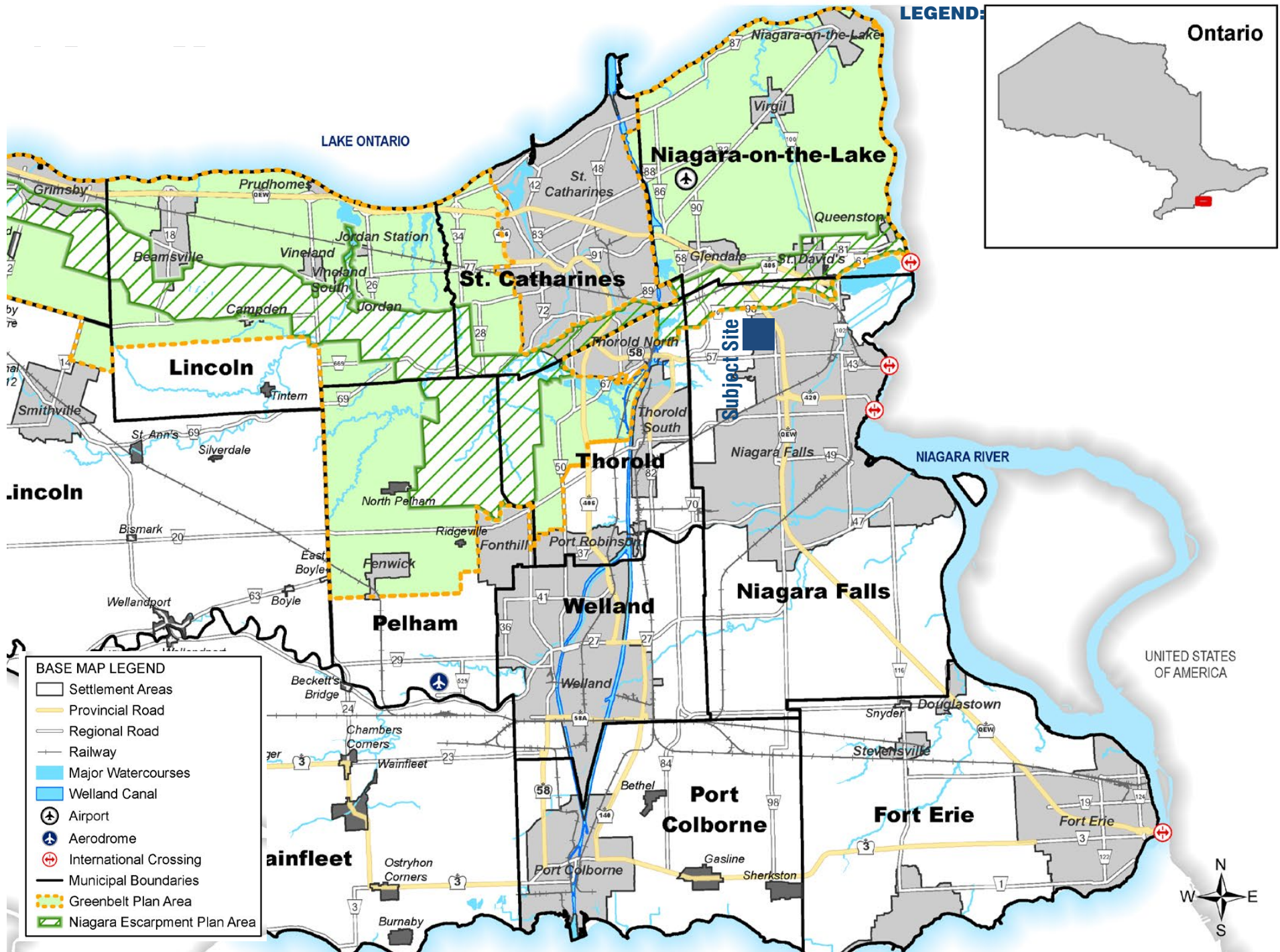


FIGURE 6: NIAGARA REGION OFFICIAL PLAN - SCHEDULE A

2.2.2 Growth Plan for the Greater Golden Horseshoe (2020)

Policy

The Growth Plan is part of the provincial planning framework that builds upon the PPS to direct how and where growth will occur within the Greater Golden Horseshoe area. Objectives guiding the Growth Plan include supporting the development of complete communities, a thriving economy, a clean and healthy environment, and social equity. The Subject Site is within the designated Built-Up Area of the Growth Plan and is approximately 2.6 kms east of the Committed GO Transit Rail Extension on Schedule 5.

Policy 2.2.1 of the Growth Plan stipulates that the majority of growth will take place within the delineated built boundary of the plan in areas that have existing or planned municipal water and waste water systems and can support the achievement of complete communities. Further to this directive, policy 2.2.2.1 sets a target that a minimum of 50 per cent of all residential development occurring annually within the **Region of Durham** will be within the delineated built-up area.

Policy Section 2.2.6, Housing, directs that municipalities will support the achievement of complete communities by planning to accommodate forecasted growth; planning to achieve minimum intensification and density targets set out by the plan; considering the range and mix of densities of the existing housing stock; and planning to diversify their overall housing stock across the municipality.

Response

The proposed development will help achieve the intensification target by providing additional residential uses while also maintaining the community character. In terms of housing mix and uses, the proposed development provides for a nine additional townhomes and two additional single-detached homes in an area well serviced by transit. The proposal will increase the supply and housing choice and supply available in the area by providing for 11 new dwelling units. The proposal is able to accommodate the needs of various household sizes, which is an essential component of complete communities. The Subject Site is also within walking distance to commercial, medical, institutional and recreational facilities, reducing the reliance on cars for future residents.

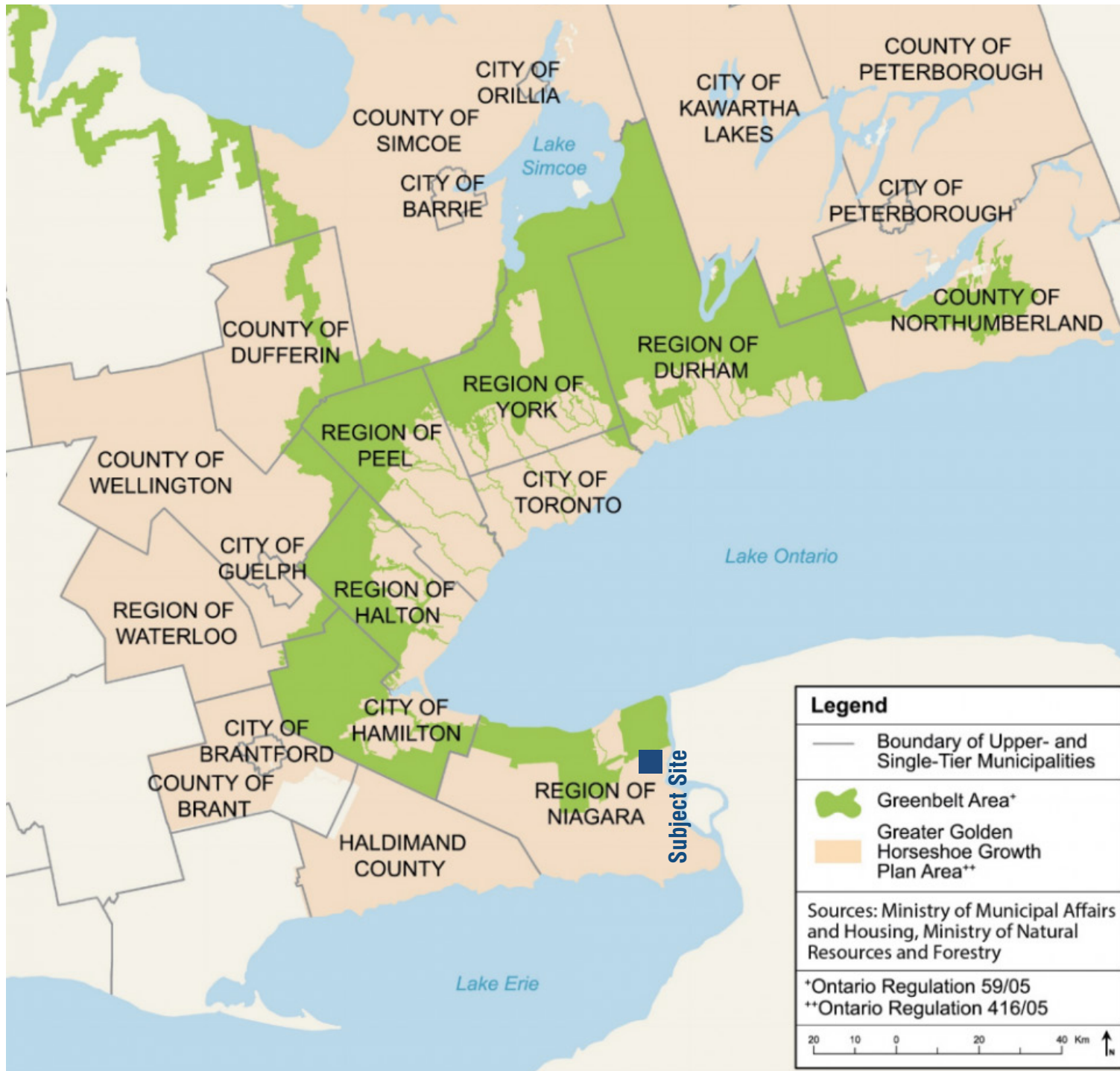


FIGURE 7: GREATER GOLDEN HORSESHOE GROWTH PLAN AREA

2.2.3. The Niagara Regional Official Plan (2022 Provincial Approved) (“ROP”)

Policy

The Niagara Regional Official Plan is consistent with the policies of the Province, and sets the land use and planning framework for local Official Plans. The policies of the Niagara Regional Official Plan guide the physical, economic and social development of the Region to manage growth and economic development, to protect the natural environment, resources and agricultural lands, and to provide long-term goals for infrastructure investment.

The Subject Site is designated as Built-Up Area under the Urban Area designation in the ROP. Per Policy 2.2.2 Built-up Areas will be the focus of residential intensification and redevelopment within the Region over the long term. The Region implements several policies to promote intensification within the Built-Up Area and directs for local municipalities to generally encourage intensification throughout the Built-up Area (Policy 2.2.2.9 and 2.2.2.10.). The Regional OP provides an intensification target of 10,100 units to be built in Niagara Falls by 2051, and that 50% of all new residential growth annually should be within the delineated built-up area.

Section 2.3 provides the Region's direction on housing. Policy 2.3.1, states that the Region encourages the provision of a variety of housing types within urban communities and neighbourhoods to serve a variety of people as they age through the life cycle. Policy 2.3.2 states that the Region encourages the development of both affordable and attainable housing. Minimum targets are set.

Response

The proposed development will help the Niagara Region to achieve the highlighted intensification targets set out by the Region. The proposed development will support intensification efforts through the provision of eleven (11) additional units within the Built-Up Area that supports the existing transit routes and active transportation infrastructure.

The proposed development will add additional, attainable housing options and units to serve those throughout different stages of the life cycle. This will be done through the provision of both single-detached dwellings and townhomes that complement the surrounding lower density neighbourhoods and local businesses. The transition in scale and density will support the overall design character and consistency of the neighbourhood.

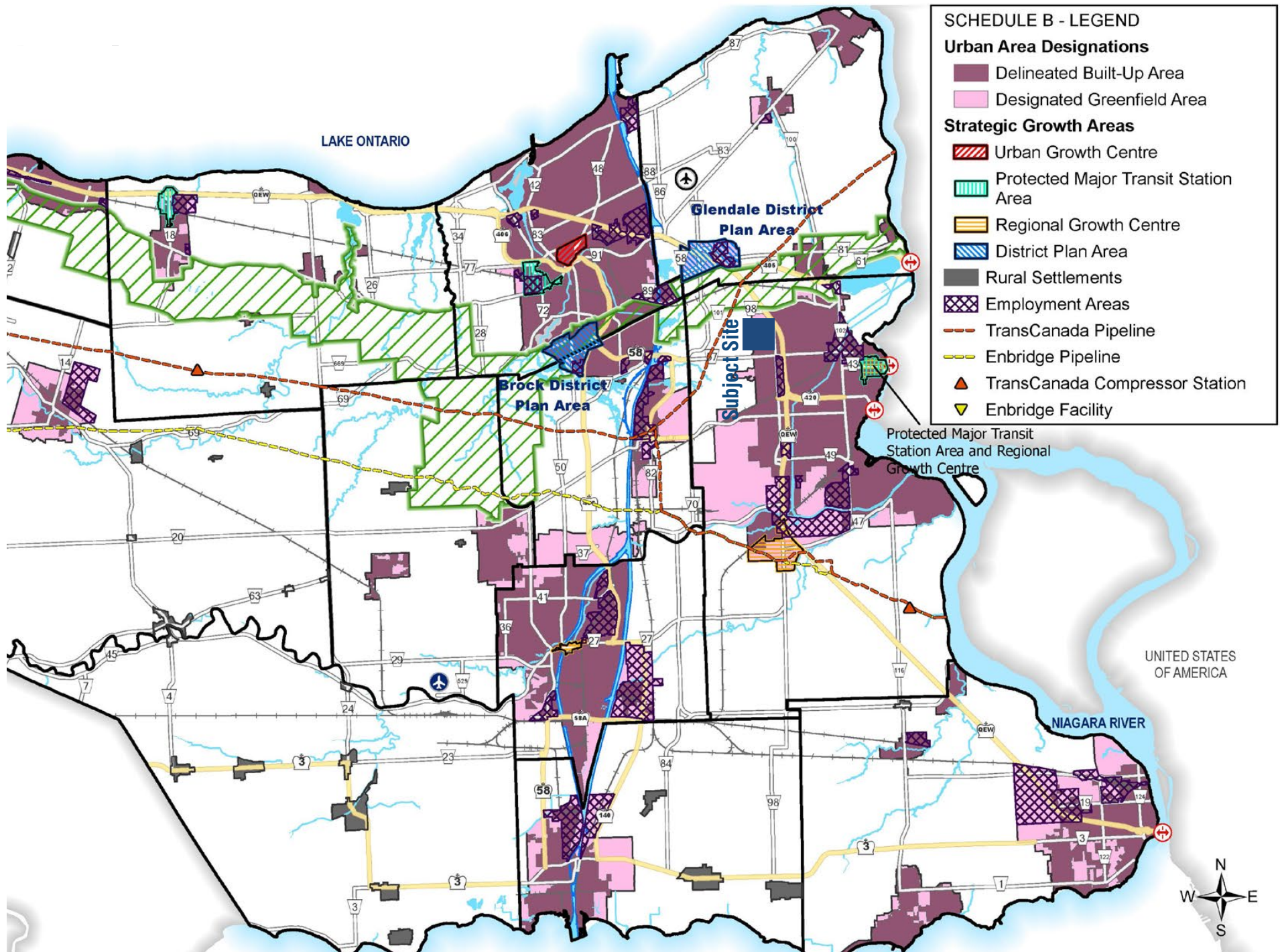


FIGURE 8: NIAGARA REGION OFFICIAL PLAN: SCHEDULE B - REGIONAL STRUCTURE

2.2.4. The City of Niagara Falls Official Plan (Office Consolidation, 2019) (“COP”)

Policy

The main purpose of the COP is to create a long-term vision for the City to guide local growth and development by designating land for various land uses and providing policies that will guide development and use of land City-wide. The Subject Site is designated as Residential, within the Built-up Area of the COP. Under the COP, the Subject Site is designated as a Residential. There is no Natural Heritage designation for the Subject Site under Schedule A-1 of the City OP. Both Montrose Road and Thorold Stone Road are classified as Arterial Roads per Schedule C. The Subject Site does not fall under the purview of a Secondary Plan. Schedule D indicated that the Subject Site are in the North-West community of Niagara Falls.

Section 2 of the COP provides Growth Objectives in Section 2, under the heading of Strategic Policy Direction. Objectives that are relevant to the proposal are as follows:

- To direct growth to the urban area and away from non-urban areas (Objective 1)
- To protect Natural Heritage Areas and their functions (Objective 2)
- To support increased densities, where appropriate, and the efficient use of infrastructure within the Built-Up section of the urban area (Objective 3)
- To meet the targets as established by the Province through the Greater Golden Horseshoe Growth Plan and through the Region of Niagara Comprehensive Review (Objective 5)
- To accommodate growth in accordance with the household, population and employment forecasts of the Region’s Comprehensive Review (Objective 6)

- To encourage alternative forms of transportation such as walking, cycling and public transit (Objective 9)
- To develop a transit and pedestrian friendly, sustainable and livable City through the use of urban design criteria and guidelines (Objective 13)

Further, Policy 1,2 discusses the City would support multiple unit developments, smaller lot sizes and innovative housing forms in order to diversify the housing typology and cost within the Built-Up Area. The development of vacant land and more efficient uses of under-utilized parcels is also noted in this Policy.

Section 5 of the City OP discusses the importance of Urban Design and its impacts on the quality of life for citizens of Niagara Falls. This section identifies the importance for new developments to maintain the height, massing and architecture of the surrounding buildings to produce a compatible relationship within the community of the development. Moreover, Policy 5.1.2 indicates that development should be designed and orientated with pedestrians in mind. Pedestrians should be able to move freely within buildings and development should be set as close to the street as possible. On this note, parking should be located in the rear or side yard of developments with sufficient landscaping.

2.2.5. Niagara Zoning By-Law 79-200

Response

The proposed development aligns with the objectives set forth by the City OP. The Subject Site is located within an urban, Built-Up area with no Natural Heritage implications. Due to this location, the Subject Site is located in a suitable area for development. As previously mentioned, the proposed development meets the targets established by the Province outlined in the Greater Golden Horseshoe Growth Plan and the Region of Niagara Official Plan.

Furthermore, the proposed development would allow for additional housing types and densities while encouraging alternative forms of transportation due to its proximity to public transportation.

The proposed development aligns with the policies discussed in the City OP by providing more efficient use of the vacant spaces surrounding the current single-detached dwelling on the Subject Site. Further, the proposal provides additional housing and different typologies to the community in an innovative manner.

Policy

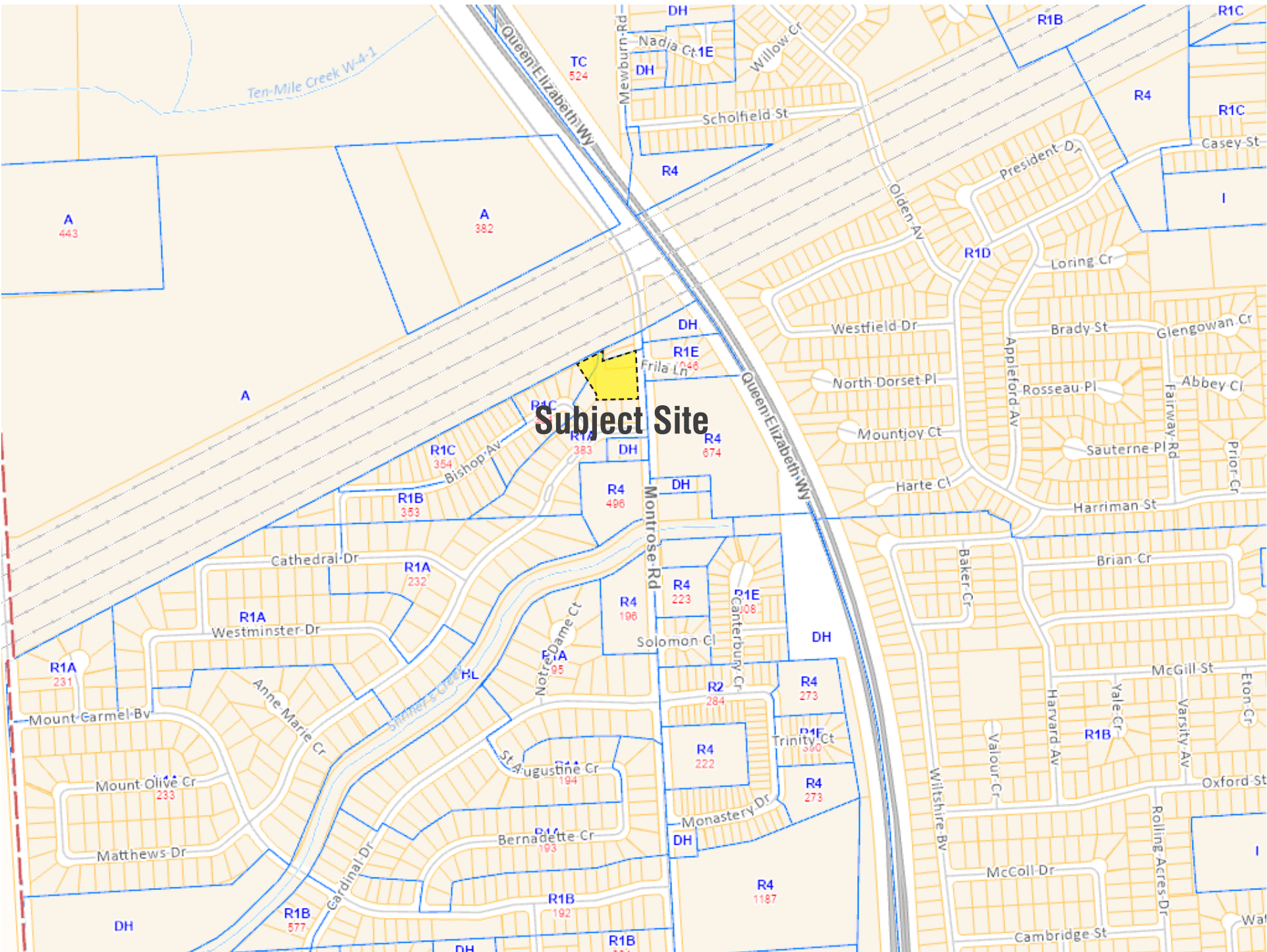
The City of Niagara Falls Zoning By-law 79-200 zones the Subject Lands as R1A-383 (Residential 1A Density Zone) & R1C-384 (Residential 1C Density Zone). The current special provisions on the Subject Site, indicate that only single-detached dwellings must be built on the Subject Site.

Response

Both Residential 1A & Residential 1C Density Zone allows for detached dwellings, but not townhomes. In order to permit the proposed development, the Subject Site must be rezoned to a Site-Specific Residential Low Density, Grouped Multiple Dwelling Zone (R4). Under this zone, one or more of on-street townhomes and single-detached dwellings are permitted.

A Site-Specific provision is needed due to the depth of the townhouse dwellings, depth of the private yards from townhouse dwellings to the exterior rear wall and lot coverage maximum being 4% higher than required for the R4 zone

City of Niagara Falls Council supported the staff recommendation to approve the application for Zoning By-law Amendment on December 12, 2023 with the By-law to be passed in January, 2024.



Subject Site

2.2.6. The Niagara Region Model Urban Design Guidelines

Introduction

The Niagara Region Model Urban Design Guidelines are intended to help facilitate development and redevelopment in a progressive manner. The guidelines contain 10 principles for smart growth which are the structural element for the urban design guidelines. They are as follows:

- Create a mix of land uses;
- Promote a compact built form;
- Offer a range of housing opportunities and choices;
- Produce walkable neighbourhoods and communities;
- Foster attractive communities and a sense of place;
- Preserve farmland and natural resources;
- Direct development into existing communities;
- Provide a variety of transportation choices;
- Make development predictable and cost effective; and
- Encourage community stakeholder collaboration.

Policy: 3a Neighbourhood Structure

Section 3a Neighbourhood Structure contains guidelines for Lot Size & Variety (subsection 3a.4) stating that lot shapes should be simple and rectilinear so as to not limit design and siting options. Corner lots should have adequate width to permit appropriate building setbacks from both streets. Subsection 3a.6 relates to Transit Supportive Design and states that neighbourhoods should provide a mix of land uses and higher densities at key locations to generate pedestrian traffic.

Response

The proposed development includes a variety of built forms including two single detached homes, a block of four townhouses and a block of five townhouses. When possible lots are rectilinear, however given the shape and size of the Subject Site, the single detached lots are asymmetrical, however remain usable and pragmatic from a development standpoint.

Figure: Standard size residential blocks (approx. 200m long and 55m deep) should achieve a variety of lot sizes and configurations.

It is possible to achieve a range of housing forms and densities, including detached, semi-detached, townhouses, and also apartment buildings (not shown) within the same block.

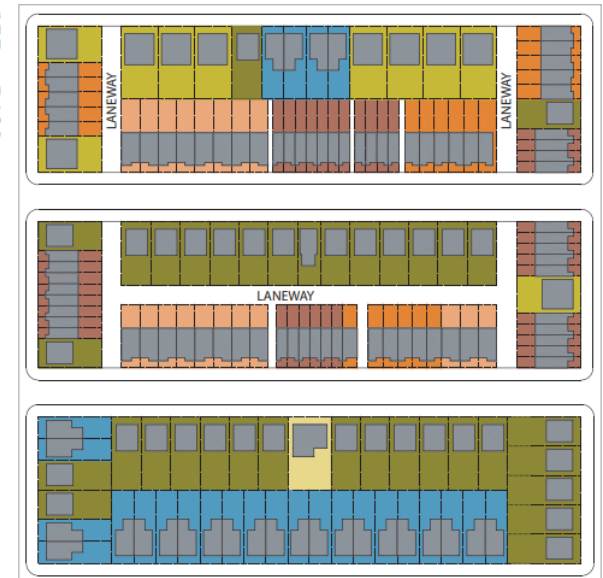


FIGURE 10: BLOCK SIZE

Policy: 3c Sidewalks & Streetscaping

Section 3c Sidewalks & Streetscaping outlines 4 Design principles to be applied including:

- Public spaces: Sidewalks should be designed as high quality public spaces, promoting active uses by residents and visitors and enhancing the pedestrian experiences.
- Scale: Sidewalks should be designed according to the function and nature of adjoining land uses.
- Safety: Sidewalks should provide safe environments and provide unobstructed pedestrian movements along the street. Key elements of safe design include lighting and buffers from fast-moving traffic.
- Connections: Sidewalks should be located so that the majority of residents are conveniently connected between destinations.

The use of Street Trees are encouraged to provide shade and comfort to pedestrians and enhance the visual and environmental qualities of the street (subsection 3c. 7). Native species for street trees should be used wherever possible.

CONFIRM

Response

Pedestrian sidewalks will be sited throughout the proposed development to encourage walking and to improve the pedestrian experience. Where possible native street trees will be planted along the sidewalks and roads.

Curb face sidewalks on one side of the private laneway are being proposed. A high degree of landscaping will be provided throughout the site. The City has not requested sidewalks along Montrose at this time as there is no connection further south of the site

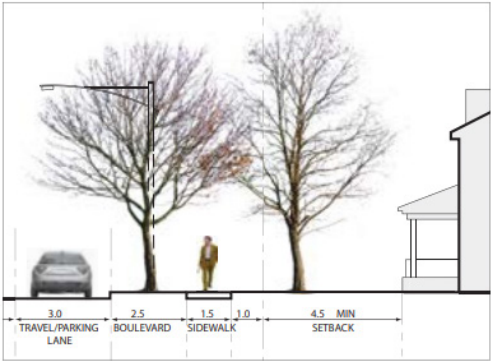


Figure: Residential area sidewalks should be at least 1.5m wide.



Figure: Sidewalks should be barrier free, and constructed of high quality and stable materials.

FIGURE 11: SIDEWALKS & STREETSCAPING

Policy: 3h Environmental Sustainability

Section 3h.1 Design Principles outlines 4 guidelines to encourage Environmental Sustainability:

1. Conserve Land: Development of the public realm should be compact. Development and intensification should occur within existing urban areas to reduce pressure on natural features, open spaces, rural and agricultural lands.
2. Conserve Energy: The design of blocks, streets, transit facilities, collocation of uses, and recycling, can help to reduce energy consumption at the neighbourhood level.
3. Reduce Water Runoff: Through the preservation of natural vegetated environments, extensive landscaping, and the appropriate use of porous surfaces, urban design strategies can reduce water runoff and improve water quality. Reducing water runoff helps control erosion, river and stream pollution, and can recharge depleted groundwater resources.
4. Promote Alternative Modes of Travel: Alternative modes of travel, such as walking, bicycling and public transit can be promoted through the design of high quality streets with adequate sidewalks, bicycle lanes, and transit facilities (bus stops, seating, etc).

Response

The proposed development will increase the density of the subject site from a single detached dwellings with the addition of 2 new single detached dwellings and 9 new townhouses. The Subject Site is within an urban, Built-Up area and will be able to capitalize on existing services and amenities in the area.

Subject Site's

The proposed development will implement sidewalks throughout the site to encourage walking and cycling. The **Subjects Sites** close proximity to existing public transportation infrastructure will further encourage the use of alternative forms of transportation.

Policy: 4a Residential

Section 4a Residential includes a series of principles and guidelines to direct the design and development of residential projects. Subsection 4a.1 includes 5 design principles:

1. **Positive Image:** A positive residential image is a key design consideration for enhancing the quality and character of the overall streetscape and neighbourhood. Housing should incorporate architectural design elements to create a positive street image. Elements such as front-attached garages or blank walls must be avoided.
2. **Context Sensitive:** The mass, scale, and architectural elements of residential buildings should be sensitive to adjoining areas. Design elements such as the height, building mass, and architectural features should complement the overall neighbourhood character.
3. **Housing Variety & Choice:** A full range of housing types (i.e., detached, semi-detached, townhouse, apartments) and tenures should be provided so as to provide options for a wide range of residents/family types. A range of housing types will address changes in market conditions and provide flexibility for people at a variety of income levels.
4. **Flexible & Adaptable:** Multiple unit and apartment housing should create opportunities for a wider range of uses, other than residential, such as homeoffice and apartments situated above street commercial.
5. **Environmentally Sustainable:** Residential development should be designed to achieve a high degree of environmental sustainability and address opportunities for solar orientation and water runoff minimization.

New housing should face adjacent streets and open spaces. Rear lotting should not be permitted unless it is required to achieve a reasonable design objective as per sub section 4a.3. Residential setbacks for front yards should have a minimum of 2.5m and a maximum of 4.5m. Subsection 4a.4 indicates that there should be a diversity of front yard setbacks on the streetscape.

4a.7 Architectural Features provides guidelines to achieve a visually rich residential building fabric that promotes a distinct neighbourhood image through the use of materials, building form, and architectural styles. Architectural expression should be varied and recognizable in its local context and quality should be consistent and building materials should be complimentary. Buildings should have consistent rhythms of similar but not identical details. Building projections such as porches, decks and canopies and stairs are encouraged.

Driveways & Garages are outlined in subsection 4a.8. Parking and garage should not be the dominant portion of the house facade. Garages located on the front side of the house should be no wider than one half of the width of the house. There should be no projections of the garage beyond the front face of the house.

Response

The proposed townhouse design includes a variety of architectural features and materials such as gabled roofs with returns, dormers on the second floor and architectural detailing around the windows and porches.

As indicated in the renderings (figure 15) the some of the garages and driveways will be paired, while others will not to introduce variety while also introducing a pattern of garages-porches-garages etc. The front faces of the building, entrances and porches protrude beyond the face of the garage doors diminishing their impact on the facade of the proposed development.

The frontages along Montrose Road will be designed to appear as the front of the house, however, the required noise fencing may create a visual barrier to the front of the units from the street

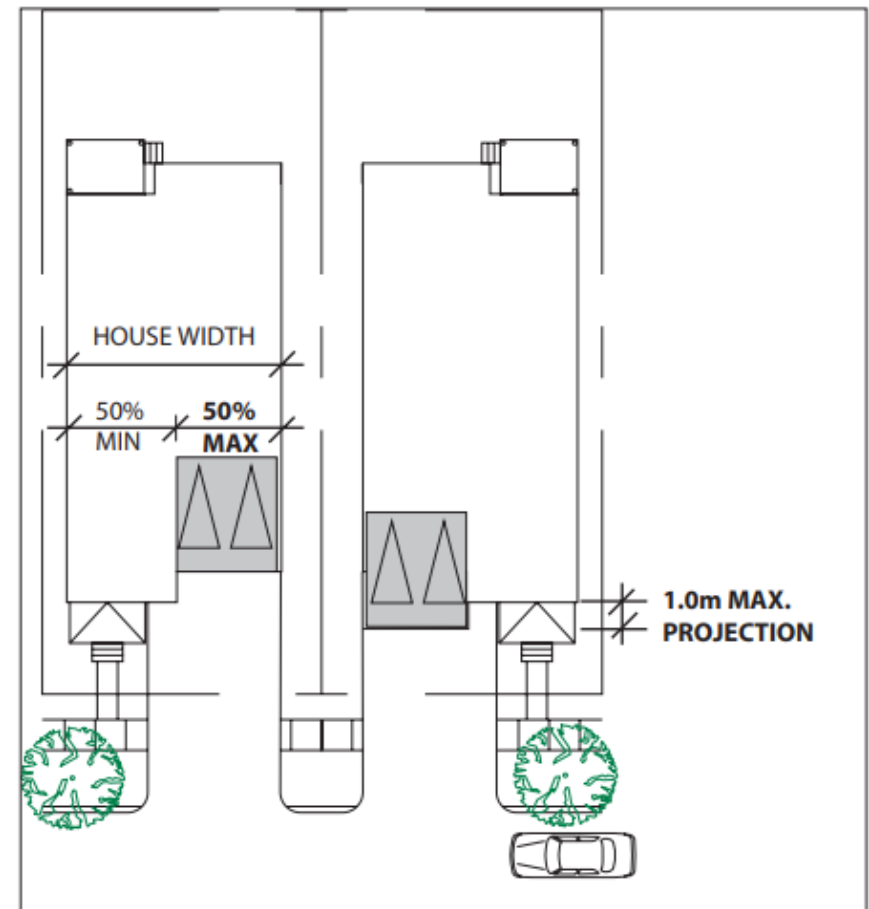


Figure: Design guideline for residential garage width and setback.

The total garage width should not exceed 50% of the total house width.
The garage should not project more than 1.0m beyond the house facade.

3 THE PROPOSAL

3.1. DESIGN PRINCIPLES

The urban design vision for the proposed development is to create an innovative and sustainable building and landscape design. Contemporary building solutions will combine with traditional and contemporary architectural styles to maintain the character and image of the community while contributing to a more sustainable future.

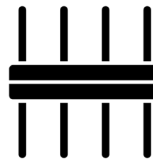
The following guidelines provide an outline of design initiatives specific to informing the proposal and conceptual design of development for the subject site. The supporting urban design principles have been summarized and outlined below for the subject site proposal.

The urban design goals and vision are to promote increased density, walkability, encourage cycling, and accommodate a range of architectural elements consistent with the existing character of the neighbourhood. The guiding design principle is to draw on the balance between existing and new, low- and medium-density dwellings.

The detailed design principles are analyzed as per the following:



SITE DESIGN



CIRCULATION & ACCESS



ARCHITECTURAL STYLE






3.2. Site Design

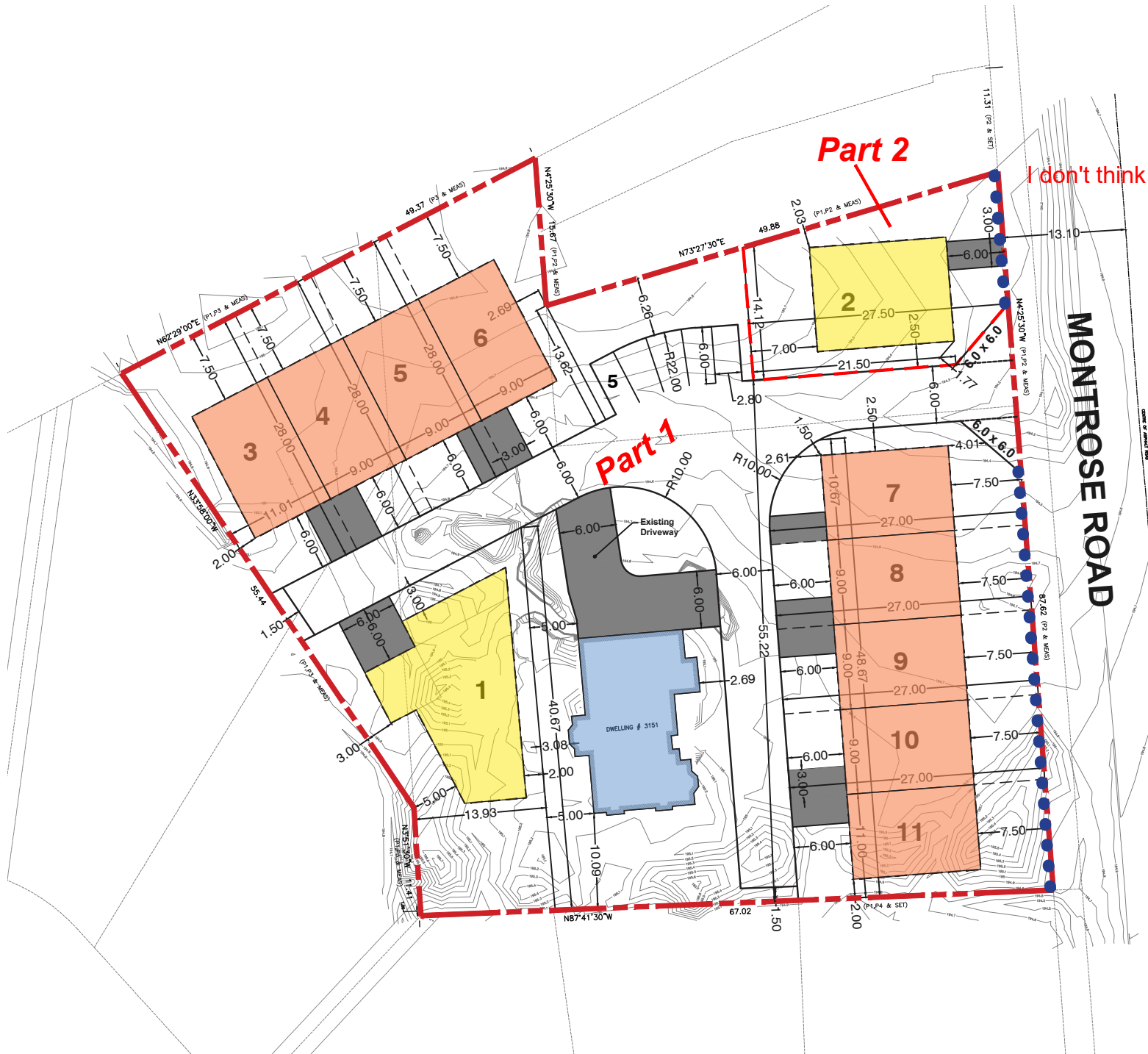
The proposed development of 2 single detached dwellings, a block of 4 townhouse units and a block of 5 townhouse units offers an increase in housing options and provides an increase in density within the surrounding neighbourhood. Townhouses provide a more compact built form and a more efficient use of land while maintaining the existing character of the area as grade-related building forms. One existing single detached dwelling will remain on the Subject Site.

The primary access to the site is from Montrose Road. The 9 townhouse units will front onto the new private road and be setback 6 metres from the curb. The townhouse units will have 7.5 metre rear yards and a minimum of 2.0 metre sideyards. One of the single detached dwellings will front onto the new private road as well and be setback 3 metres. The second single detached dwelling will have frontage along Montrose Road.

A proposed soundwall along Montrose Road will reduce noise from Queen Elizabeth Way which is approximately 150 metres east of the Subject Site and produces substantial traffic noise.

LEGEND

-  Subject Site
-  Existing Single Detached-Dwelling
-  Proposed Townhouse Block
-  Proposed Single Detached Dwelling
-  Sound Wall



I don't think this lot gets a wall.

FIGURE 13: LAND USE

3.3. Circulation & Access

As previously mentioned the primary vehicular access is via Montrose Road. A T-shaped private road within the site will provide access to the existing single detached dwelling, townhouse blocks and one of the new single detached dwellings.








Front entrances and garage entrances alternate along the front facade to break up the front elevation of the proposed development. The proposed front porch extend beyond the building face, making it a focal point of the building design while accentuating the front entrances over the garages. The inclusion of dormers on the third level will help breakup the large roof. An elevated wood deck in the rear yard will provide private outdoor space, as will the ample backyards.

Five visitor parking stalls will be provided on the north side of the private road between the block of four townhouses and the new single detached dwelling along Montrose Road.

SIDEWALKS?

Sidewalks on one side of the private road. No sidewalks along Montrose were requested because they don't connect to anything. Potentially would come later when the lands to the north are developed

LEGEND

-  Subject Site
-  Vehicular Circulation
-  Pedestrian Circulation
-  Front Entrance/Porch
-  Garage Entrance
-  Driveways
-  Visitor Parking

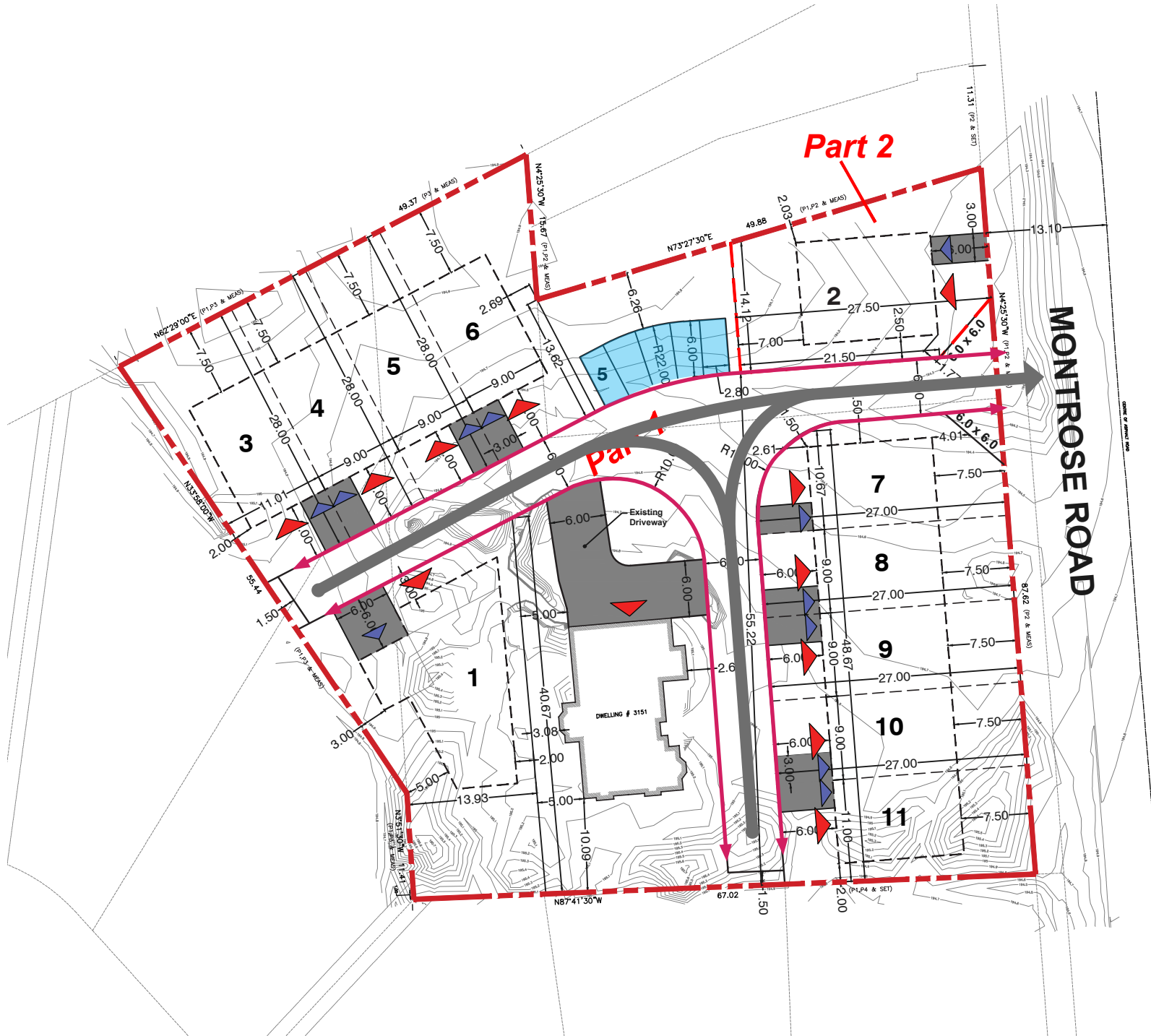


FIGURE 14: SITE DESIGN

3.3. Architectural Style

The proposed townhouse units are designed in the craftsmen style and include a architectural features and materials such as gabled roofs with returns, dormers on the second floor and architectural detailing around the windows and porches.

Driveways and garages will be paired when possible to mitigate the impact they have on the front elevation of the building. Garages will not occupy over 50% of the units frontage. The front face of the garages will be setback from the front face of the entryway and porches to further reduce the prominence the garages hold on the design of the townhouses.

Front porches will extend beyond the front face of the building and include various architectural detailing to emphasize the entrances. The inclusion of dormers on the third level will help breakup the large roof.



FIGURE 15: ARTISTS RENDERING

CONCLUSION

The proposed development conforms with the Municipality of **Clarington's General Architectural Guidelines** and reinforces the commitment to create a sustainable community in town of **Bowmanville**. Urban design and architectural strategies will be incorporated in order to promote a safe, pedestrian-friendly and comfortable built environment. Additional attention and enhancements will be made to the townhouse elevation and garage placement. The established community character will be expanded and enhanced through complementary building designs strategies.



Not niagara falls