

P R O S P E R I T Y



C U L T U R E

HISTORIC DRUMMONDVILLE NIAGARA FALLS

CONCEPTUAL URBAN DESIGN GUIDELINES

H E R I T A G E



C O M M U N I T Y



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Figure 1: Adjacent Neighbourhoods and Major Circulation Routes

Building on the Strengths of adjacent Neighbourhoods and Major Circulation Routes



Figure 2: Clifton Hill Streetscape



Figure 3: Niagara Parks Commission Streetscape



Figure 4: Fallsview Casino Entrance

1 INTRODUCTION

The City of Niagara Falls retained RCI Consulting in association with Marshall Macklin Monaghan and du Toit Allsopp Hillier, to develop with local stakeholders a Community Improvement Plan (CIP) for the area surrounding Main Street and Ferry Street in Niagara Falls. Through the development of the vision as part of the first phase of the Community Improvement Plan study, feedback from the public and staff was favourable relating to the renaming of the study area as “Historic Drummondville”. Along with the Land Use Plan, the Conceptual Urban Design Guidelines are a key component that will guide implementation of the CIP. These Urban Design Guidelines will help to ensure that the vision developed for the Historic Drummondville area, is realized as the CIP is implemented and the rehabilitation and redevelopment of the area takes place over time.

1.2 REPORT PURPOSE

This report is intended to assist the City and the Main and Ferry Business Improvement Association (BIA) in their review and response to intensification, infill and new development proposals. The Guidelines provide a clear vision and framework for appropriate relationships between buildings, streetscapes and public spaces. The Guidelines promote crime prevention by promoting more “eyes on the street” and removing the physical opportunities for crime in keeping with the principles of CPTED (Crime Prevention Through Environmental Design). This report addresses the unique challenges and opportunities of Historic Drummondville such as:

- Building upon the success of the Fallsview casino based retail district;

- Adopting guidelines that are respective of the historic nature of Historic Drummondville while promoting long-term intensification;
- Undertaking design review (facade, signage and massing) for development approvals; and
- Encouraging public and private partnerships to create integrated development.

1.3 KEY OBJECTIVES

The urban design guidelines are intended to address the following key objectives:

- Preserve and highlight the unique character of Historic Drummondville;
- Encourage new development while preserving and enhancing existing high quality built form;
- Reinforce the identity of the area as a predominantly low to mid-rise main street retail area;
- Expand and improve the quality and comfort of the public realm (streetscape and open space network) to enhance accessibility and promote year round use;
- Reduce crime and vandalism by improving the physical / social environment through enhanced urban design;
- Promote the introduction of higher order retail and service type businesses in area;
- Encourage redevelopment / revitalization of underutilized sites and their enhancement of the public realm; and
- Protect and strengthen surrounding established residential areas.

2 THE CHALLENGE AND OPPORTUNITY

The Historic Drummondville neighbourhood has evolved significantly since it was a site of the war of 1812. It was the first colonized settlement in the Niagara area, with farmers' fields stretching down from it to the Niagara River. Twenty-five years ago it was the local neighbourhood hub and "Main Street" to the south Niagara Falls community. Main Street is now characterized by a high business vacancy rate, a number of deteriorating buildings, and social problems that have transformed this once thriving commercial area into an area that is in need of community improvement.

STRENGTHS TO BUILD ON

As the location of Niagara Falls' original settlement, Drummondville contains many historic sites including the Battlefield, the original Town Hall and a stop for the 'underground railroad'. Vestiges of these and more recent history remain as key assets that could form the basis of a heritage theme for renewal of the neighbourhood as well as the larger Niagara Falls community. The market and the activity associated with markets should mark and punctuate the experience of the street.

Historic Drummondville is also surrounded by thriving retail and commercial districts. The Fallsview District and casinos draw large numbers of tourists to the area. Lundy's Lane is also a key destination for tourists as well as city and regional residents. As such, Drummondville is well positioned to capture its share of visitors to the area, provided that it can provide a complementary visitor experience. This could include

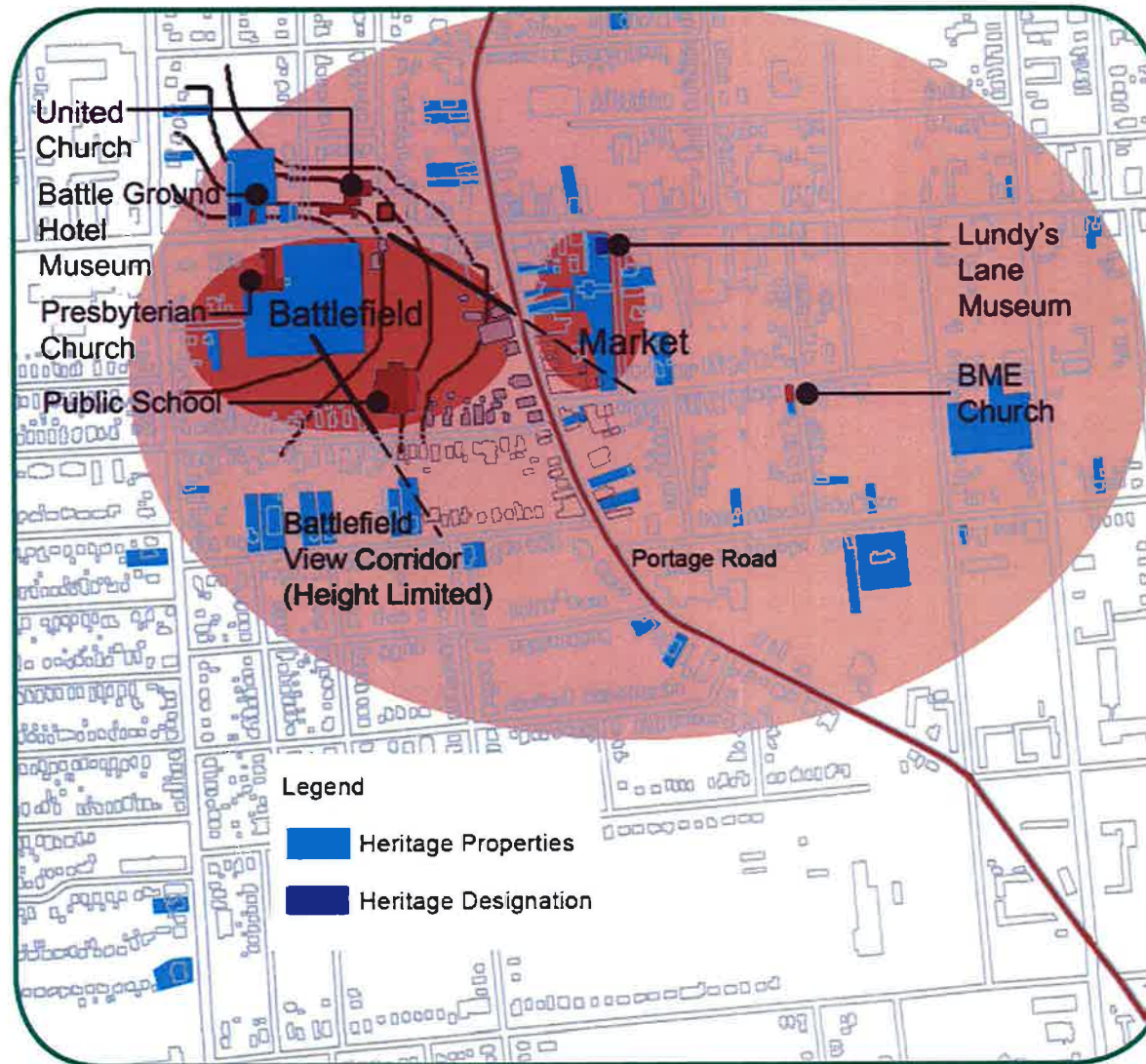
features and programs building upon the existing heritage museum as well as interpretation of the historic sites. The streets and public spaces could build upon the historic buildings and walkable, human scale of the shopping areas. The market could be reinvented to host a variety of retailing and cultural market opportunities.

The area could also become home to a new resident community. The neighbourhood is well located in relation to health care services, seniors' community facilities, transit services and the Fallsview Tourist district. The surrounding neighbourhoods are stable and provide a good setting for more intensified forms of residential development for empty nesters, seniors, and the employment community serving the casino and hotels. These neighbourhoods combined with intensified development along Main and Ferry Streets should build on the concept of "eyes on the street" to decrease crime and improve the feeling of security in the area.

The challenge for Historic Drummondville is to see beyond the current situation, take advantage of its strengths and to put programs in place that will encourage redevelopment. Public investment in heritage resources, streetscapes, open spaces and appropriate community facilities will contribute to a climate of investment and renewal in support of a corresponding private sector commitment.

These guidelines are intended to help implement the vision for the area outlined in the CIP by shaping the physical planning and design of the public and private realms of buildings, streetscapes, and open space.

Figure 5: Heritage Resources - including Recommended area of Battlefield View Corridor with Height Limitations (see Land Use Plan)



In promoting Historic Drummondville as a distinctive neighbourhood with a strong sense of place, the community should:

- promote a flourishing mixed use neighbourhood which is interconnected through the local street network to adjacent neighbourhoods;
- draw upon its strong structural, topographical, historic and cultural features in its development;
- use adjacent stable residential areas to support revitalization of Historic Drummondville; and
- locate community facilities in the area to maximize their focal nature in creating anchors for development.

3 URBAN DESIGN GUIDELINES

The Urban Design Guidelines are organized and presented under the following general categories:

- Heritage Theme – a concept for developing a heritage theme for the neighbourhood;
- Gateways and Landscaped Entry Points - enhancing the entry points and transitions to Historic Drummondville;
- Streetscapes - guidelines for components of the sidewalk and boulevards;
- Streetscape Furnishings - guidelines for pedestrian paving, lighting and sidewalk furniture;
- Public Space, Nodes and Linkages - guidelines for key open spaces and the off-street linkages between them; and
- Built Form - guidelines for building rehabilitation, renewal, redevelopment and development on vacant sites (intensification).

3.1 HERITAGE THEME

An essential requirement in developing a unified feel to a community is to establish a streetscape design theme. The theme should reinforce the best of what already exists. In a place with strong historic identity such as Historic Drummondville, an historic or renovation theme is appropriate.

Main and Ferry has such a mix of architectural styles that there is no single common architectural denominator. There is the story of Historic

Drummondville, the Battle of Lundy's Lane from the War of 1812 and the historic Chippawa Portage trading route to build upon. Main Street is the historic 'main street' of Niagara Falls.

The buildings of the area were constructed in the 1850s. By 1910 a streetcar was running along Main Street. Ferry Street and Lundy's Lane are commercial streets for their length. Approaching the intersection of Main and Ferry, shop fronts become more or less continuous along the Ferry Street sidewalks. Main Street is a commercial street near its intersection with Ferry Street, becoming more varied with residential house forms beyond the commercial core.

As a force that unifies the stylistic mix of buildings, the streetscape design should reflect elements of the architectural styles in the area, while retaining a design integrity of its own. Each streetscape element should therefore retain a historic sense of order and use of materials while expressing this in a contemporary way. This will be discussed in further depth in Streetscape Treatments chapter.

While there are several heritage resources in the Historic Drummondville area, as shown in Figure 5, at this time we do not recommend designating the community improvement project area as a heritage district. We favour a prescriptive approach, to a restrictive one. The disadvantage of a heritage district designation is that it could actually discourage the rehabilitation of historic buildings in the area. We recommend that the issue of a heritage district designation be reviewed once the physical rehabilitation and improvement of the area has progressed (i.e., in 5 to 10 years).

Figure 6: Main Street, Historic Drummondville, Niagara Falls, c 1910



Figure 8: Port Hope, Ontario



Figure 7: Public Art Guideline - Brampton, Ontario

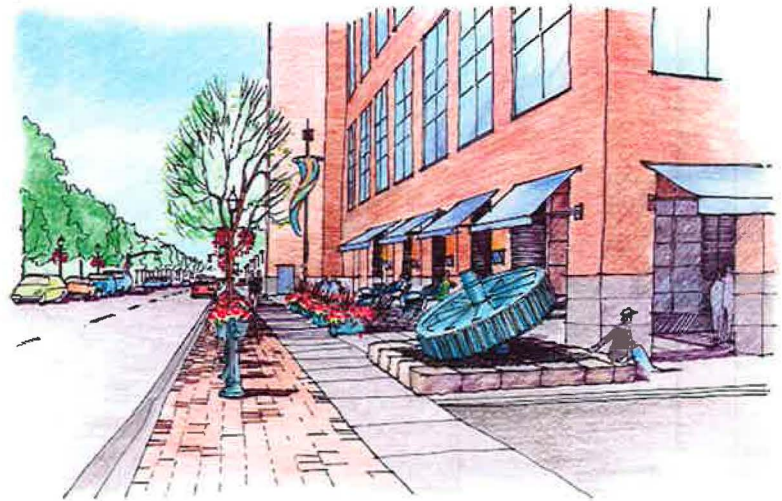


Figure 9: Cobourg, Ontario





Figure 10: 1850: Fralick's Towers

3.2 GATEWAYS AND LANDSCAPED ENTRY POINTS

A Gateway is the point of transition from one district to the next. The Historic Drummondville Gateways are the points of transition from Lundy's Lane, Fallsview and Clifton Hill to or from Historic Drummondville. As people enter, the unique identity of the area should be visible and apparent at the thresholds. Although actual gates are occasionally constructed for this function, these are often disappointing and unconvincing. Building upon existing heritage assets or unique features is a far better method of reinforcing the identity of an area. A public art study could be undertaken to aid in this enhancement. A suggested approach to reinforce gateways at the three principle points of street access to the area is described below.

Drummond Hill Gateway - should continue to identify and recall the historic battlefield to the passerby. The existing relief carvings on the retaining wall are an excellent step in signaling the entry from Lundy's Lane. Rather than an over the road gateway, design features that should be considered include elements that visually enhance the topography of the site such as:

- closely spaced flags/banners which "march up the hill";
- a gracious pedestrian stair/entrance to the Drummond Hill Battlefield;
- trees, signage and streetscape furnishings used on both sides of the street indicating that the battlefield extends over the entire hill top; and
- a rebuilding of historic tower elements.

Figure 11: Current Condition - Limited Access to Battlefield Hill



Figure 12: Precedent - Stair Gateway to Historic Site, U.K.



Figure 13: Current Condition - Confused Intersection



Murray and Main Streets Gateway - should extend from Murray Hill at Stanley Avenue, allowing the enhanced streetscape from the Fallsview Tourist District to extend along Main Street. The existing closure of Murray street at Main Street provides an opportunity to reprogram this former right-of-way as a landscaped entry point that beautifies this intersection and highlights this entrance to Historic Drummondville. The existing closure of Main Street at Stanley Avenue has diverted traffic from Historic Drummondville. Appropriate signage, landscaping, and built form will assist in improving connectivity between Historic Drummondville and the Fallsview Tourist District, as well as other points south. This linkage should emphasize the historic portage route, and curving alignment of Main Street towards Drummond Hill. Repetitive street elements (i.e., historic lights

Figure 14: Precedent - Built Form Creates Interest and Direction



Figure 15: Current Condition - Lacking Definition and Threshold



and banners) rounding the corner from Murray Street to Main Street and interpretative features at the closure on Murray Street could mark the gateway. The larger issue of reinforcing the connection between Stanley Avenue and Main Street should be reviewed as future planning and building initiatives are undertaken within both the Historic Drummondville and Fallsview Tourist District.

Gateway from Clifton Hill - should signal the transition to Historic Drummondville by taking advantage of the Hydro corridor as a landscape and open space opportunity. Unique streetlamps, landscaping, furniture and possibly banners should also distinguish Historic Drummondville from Clifton Hill.

Figure 16: Precedent - Low Level Landscape to Define Hydro Corridor



Figure 17: Existing Condition - Billboards detract from Streetscape



The rise up Drummond Hill signals the topographic landmark ahead. This is complemented by the Old Town Hall (Lundy's Lane) Museum which could be illuminated at night and be further enhanced by the forecourt and entrance to Sylvia Place Market behind. Several development opportunities flank this end of Ferry Street and could create an inviting pedestrian streetscape where there are now vacant lots. To the north, the Hospital precinct anchors Historic Drummondville and could be augmented with streetscape enhancements and possible signage.

3.3 STREETScape DESIGN

There are two main thoroughfares in Historic Drummondville - Main and Ferry Streets. Both streets should be improved, including alterations to the size and accommodation of the sidewalk areas; and improvement of the sidewalk elements such as materials and furnishings. Further, various combinations of these factors can differentiate and strengthen the character of different sub-areas or precincts within Historic Drummondville.

GENERAL DESIGN PRINCIPLES

The following are general overall principles that should be applied to all streetscapes:

- The major visual impact of the gateways should be oriented towards the car drivers and transit;
- Sidewalk widening or other streetscape improvements need to be balanced with vehicular space needs;
- Shelters and seating should be coordinated with transit stops and linkage nodes;
- Landscaping should be used to create presentable street edges to parking lots and vacant sites as an interim improvement measure; and
- As new development occurs, access points across sidewalks should:
 - be minimized and consolidated with other adjoining access points to minimize pedestrian and vehicle conflicts; and
 - be coordinated with other improvements to the sidewalk environment.

Figure 18: Existing Condition - Low Quality Streetscape



FERRY STREET

Ferry Street has narrower sidewalks than Main Street. When combined with busier vehicular traffic, the streetscape is not a particularly desirable pedestrian environment. Sidewalk space on Ferry Street should be increased where possible to improve the accommodation and comfort of pedestrians. The existing traffic lanes appear to be generous and there appears to be an opportunity to narrow the road surface somewhat in order to widen the sidewalks. Additionally, the existing parking or turning lane might be given over to sidewalk space to create additional room for pedestrians and street furniture at street crossings or special places such

Figure 19: Existing Condition - Incohesive Architectural Style



as the area in front of the Museum. Traffic movement and parking should be studied to reduce road space in favour of pedestrian sidewalk space.

Since Ferry Street is essentially a continuation of Lundy's Lane to the west and Victoria Avenue to the east, street furnishings and landscaping should be distinctive and different from these adjoining streetscapes. Uniformity and procession of distinct streetlights and trees together with banners and other regalia will be the principal elements useful in creating this distinction.

Figures 20 & 21: Current Ferry Street Streetscape (top), with Possible Enhanced Pedestrian Realm (bottom)



Figure 20 shows the existing Ferry Street streetscape, while Figure 21 shows what an enhanced Ferry Street streetscape could look like. Since Ferry Street is a commercial street, the streetscape should be designed in such a way as to allow future buildings to be built right next to the sidewalk. A continuous, pedestrian related street frontage of shops and other active uses will be the best way of enlivening the street environment and creating a people place that is distinct from the “arterial commercial” nature of Lundy’s Lane and the entertainment and tourism focus of Victoria Avenue. This pattern will also heighten the importance of open spaces that will remain such as the hydro corridor landscaping, the former Town Hall forecourt and Drummond Hill Open Space.

MAIN STREET

As previously identified, Main Street has the varied character of a traditional shopping street near its intersection with Ferry Street, and a more residential quality farther south resulting from the original pattern of single family houses. In general, sidewalk space is more generous than on Ferry Street, and there is more of a village character to the street where buildings are set back with front yards. Approaching Ferry Street, Main Street becomes more urban as should the streetscape design. Where the street opens to front yards, the character should shift to a more landscape intensive approach. Here, trees could be located in grass boulevards and beyond the sidewalks creating a pedestrian friendly green link to the Fallsview Tourist District. Minor adjustment to the roadway width could further enhance the sidewalk environment and improve planting opportunities.

Figure 22: Existing Section - Intersection of Ferry at Main

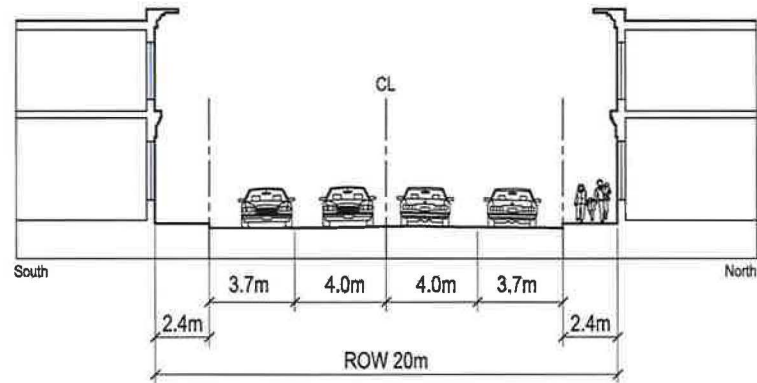


Figure 23: Proposed Section - Intersection of Ferry at Main

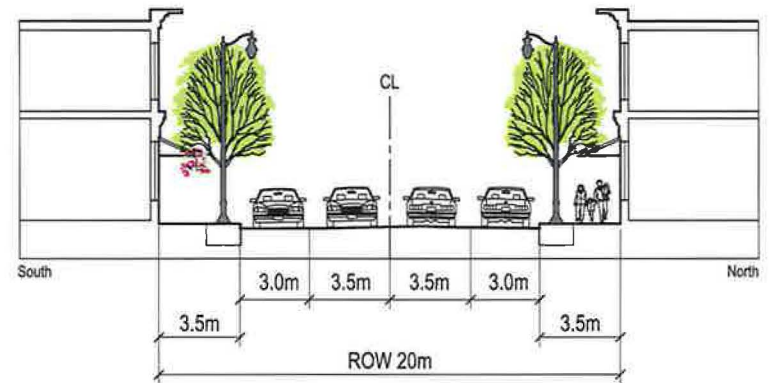


Figure 24: Existing Section - Ferry Street at Hanan

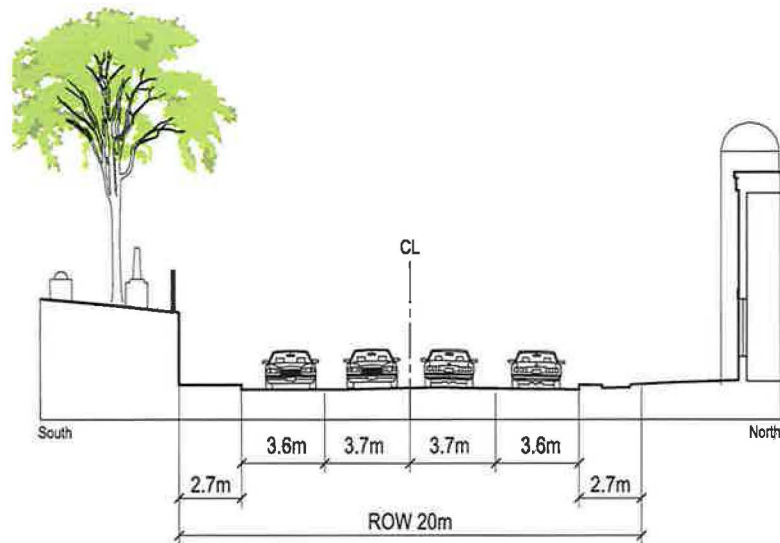


Figure 25: Proposed Section - Ferry Street at Hanan

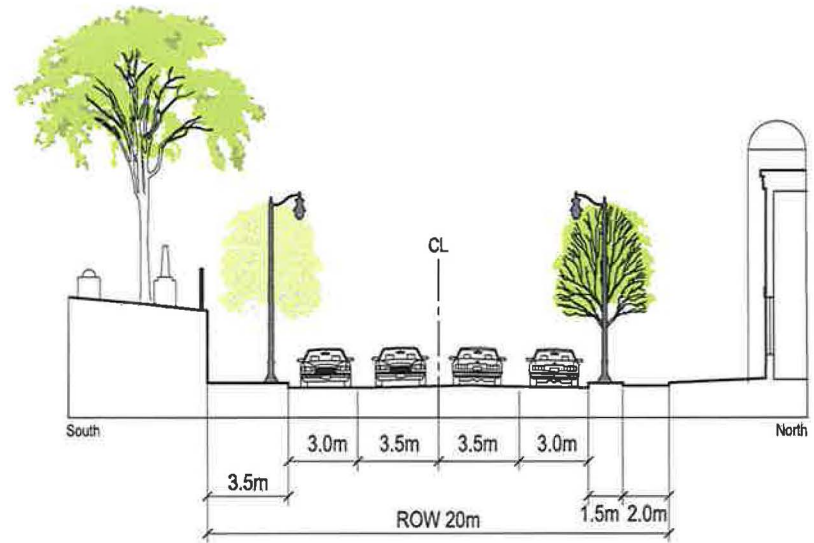


Figure 27 shows the existing Main Street streetscape, while Figure 28 shows what an enhanced Main Street could look like. Detailed design for road improvements should be undertaken in conjunction with the adoption of the CIP, and should reflect both the Conceptual Urban Design Guidelines and the site specific detailed urban design scheme being developed by Paula Berketo for the Main and Ferry Business Improvement Association.

Figures 27 & 28: View from Landscaped Entry Point at Main and Ferry with image of Current Streetscape (top), with Possible Vibrant Streetscape (bottom)



Figure 26: Precedent - Converted Residential - Niagara-on-the-Lake



Figure 29: Existing Section - Main Street at Intersection of Main and Ferry

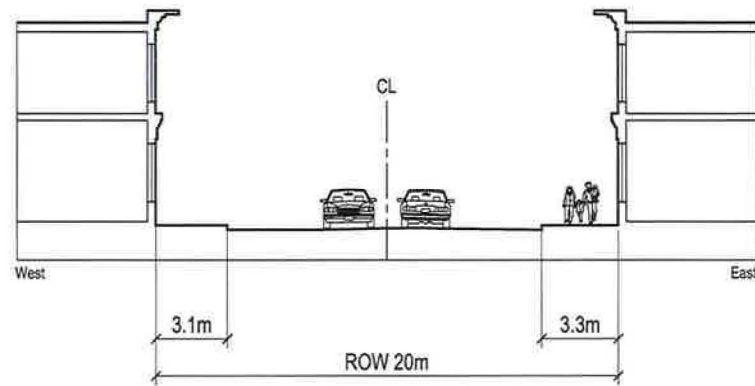


Figure 30: Proposed Section - Main Street at Intersection of Main and Ferry

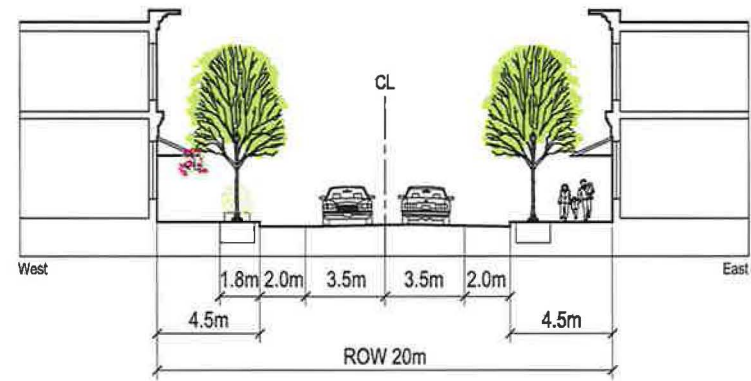


Figure 31: Existing Section - Main Street at Barker Street

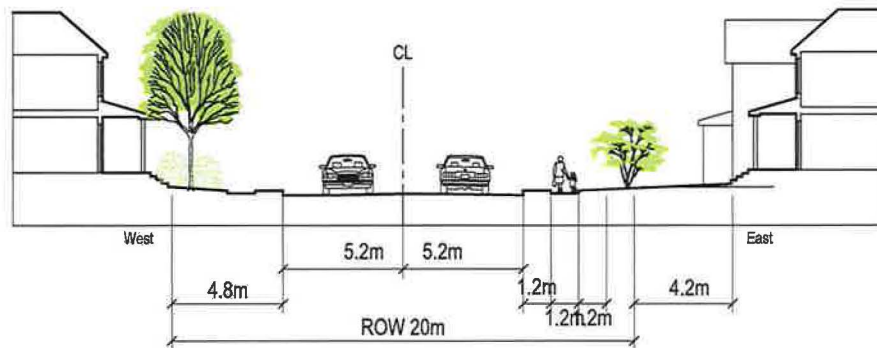


Figure 32: Proposed Section - Main Street at Barker Street

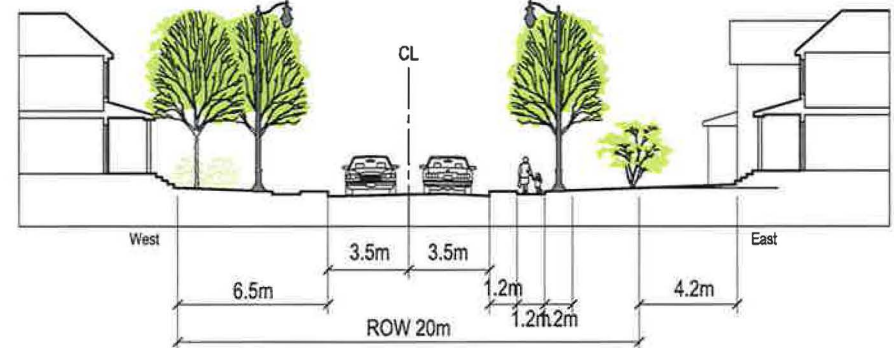
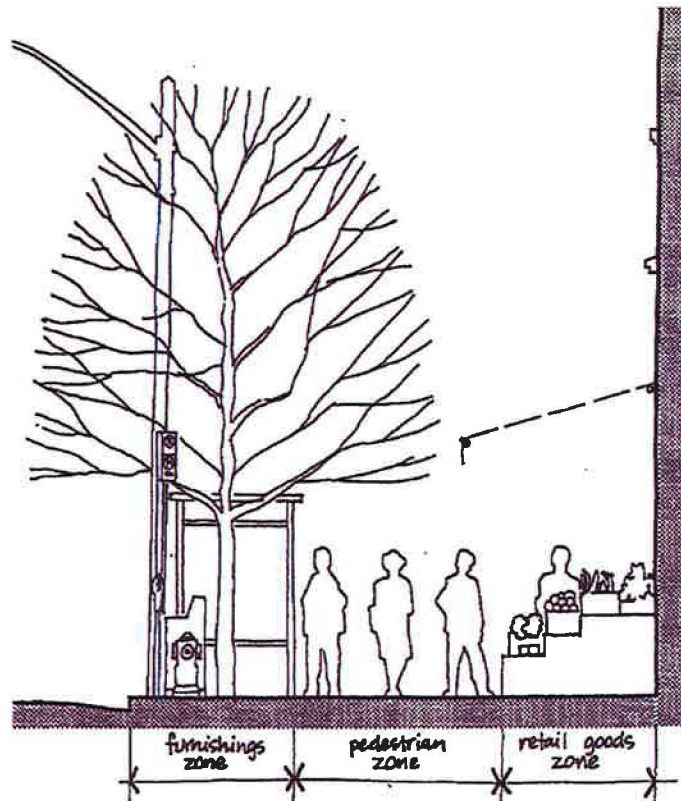


Figure 33: Sidewalk Zones, Typical Section



3.4 STREETScape FURNISHINGS

LOCATING ELEMENTS IN THE SIDEWALK

A key principle of streetscape design is grouping of the sidewalk elements in a boulevard “furnishings zone”, between the road curb and the main walking surface. Since trees, poles, signs, hydrants and various utility boxes occupy this space, other elements should also be included here to keep the central zone as free as possible.

A second sidewalk zone is the area immediately next to shop fronts for displaying goods, or for sidewalk cafes. These activities enliven a street and should be encouraged where space permits. Although there are no definitive standards for the size of these functions, the furnishings zone can usually be accommodated in 1- 1.5 m; the walking zone in 2-2.5 m; and display or café space in the remaining space. Where licensed dining occurs, a fenced area will need to be slightly wider to permit passage around tables and chairs.

LIGHTING

Street lighting and the pattern of fixtures during the day are important for both public safety and the character of the street. Although this study does not recommend a specific fixture selection, general design parameters are discussed.

Both Main and Ferry Streets are relatively narrow and as such offer the opportunity to achieve required lighting levels with either shorter pedestrian scaled fixtures or taller, normal streetlamps. Both scales of

fixture are available with heritage styling. The taller lamps provide greater opportunities for banner display and as such might be an appropriate choice for Ferry Street. This scale of historic lamp would also differ from the street lighting of Victoria Avenue and Lundy's Lane, thus distinguishing Historic Drummondville from adjoining precincts.

For "Main Street" a pedestrian scaled fixture would be more appropriate. These lights are approximately 4-5 m tall and would be more compatible with the scale of the residential house form section of the street. As well, the pedestrian scaled lights are appropriately scaled for use in the market area and connecting laneways.

Sustainability and light pollution should be taken into account. In this regard, the fixtures should direct light to the ground and have an efficient lamp type. Although slightly more energy consumptive, metal halide or other white light lamps are considered safer in respect of colour rendition and face illumination.

Feature lighting at the intersection of Main and Ferry, the Lundy's Lane Museum and other sites could be festive and draw attention to unique features of the area. Festive lighting might also include seasonal tree lights and stringing across the street.

For additional guidelines regarding lighting, see also Section 3c. 10. Lighting in the Niagara Region Model Urban Design Guidelines.

Figures 34 & 35: Street Light and Pedestrian Light Fixture Options



PLANTING

The importance of trees and planting in the streetscape cannot be overstated. Great trees make great streets. With reference to Regional road standards, we propose creating sufficient sidewalk and boulevard space to accommodate a row of primary street trees on each side of the street. With the increased sidewalk widths, the conditions of tree planting are improved, which in turn will promote a gracious sidewalk canopy.

Species that should be considered (from smaller to larger) include Bradford Pear, Linden, Honeylocust and Maple.

See also Section 3c. 4 Collector Street Guidelines in the Niagara Region Model Urban Design Guidelines; and Section 3c. 7 Street Trees in the Niagara Region Model Urban Design Guidelines.

SIDEWALK AND CROSSWALK PAVING

Pedestrian pavements should provide a smooth surface that is both durable and distinctive. Complementing the proposed historic streetscape theme, a warm traditional brick or brick in combination with concrete paving would be appropriate. These materials span all historical periods and a regular, formal pattern brings to mind historic promenades. Alternatively, unit paving that refers to the limestone geology of the escarpment and local building materials would also be fitting. The use of unit or brick paving may also be restricted to the “utility zone” next to the roadway curb of the sidewalk maintaining the walking zone and crosswalks in smoother concrete. Except for crosswalks, roads should remain asphalt. Shared vehicular and pedestrian laneways could also be enhanced with brick or unit paving. The transit hub area on Main Street is a prime example of an area that could benefit from enhanced lighting, paving, and street furniture.

STREET FURNITURE

The street furniture makes an important contribution to the streetscape theme in providing a consistent and unified appearance. The usable area of sidewalks will be affected by the placement of street furniture such as

bike stands, hydrants, planters, mailboxes, litter bins, street signs, trees and poles.

Many of these furnishings tend to be clustered at street corners adding to street congestion. When possible, these elements should be located away from the street corner, particularly news boxes. As well, provision within new building development for litter containers, telephones and mailboxes should be considered and encouraged.

While advertising on street furniture is often used to defray the costs of the hardware, this should be carefully considered not to overwhelm the visual characteristics of the furnishings. A common practice is to restrict advertising to one or a few pieces (i.e. bus shelter, information kiosks) in return for supplying all of the furnishing needs.

All street furniture should be in keeping with the feel of Historic Drummondville. Examples of heritage style furnishings are illustrated opposite. Table 3.1 also summarizes the various furnishing elements and notes additional selection and placement criteria.

See also Section 3c. 9 Street Furniture of the Niagara Region Model Urban Design Guidelines; and Section 3c. 8 Transit Infrastructure of the Niagara Region Model Urban Design Guidelines.

Figure 36: Precedent -Historic Family of Street Furniture and Directories for Fort York Neighbourhood in Toronto

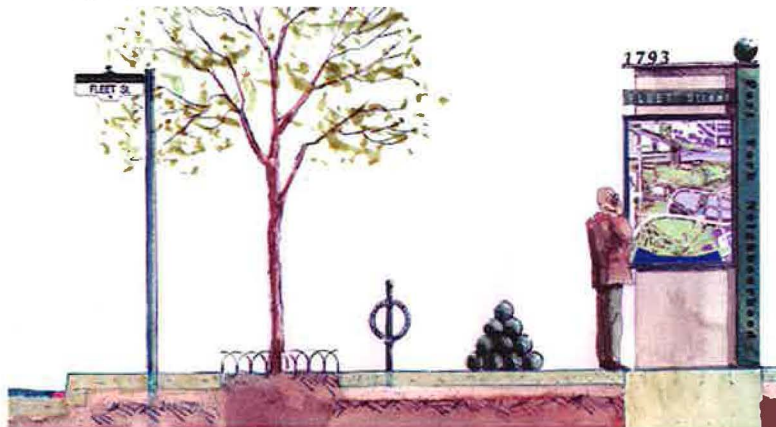


Figure 37: Historic Pedestrian Lights and Fabric Awnings



Figure 38: Historic Style Newsboxes and Litter Bin



Figure 39: Historic Style Benches and Fabric Awnings



Figure 40: Boston's Freedom Trail identified by brick



Table 3.1 - Streetscape Furnishings Guideline

STREETSCAPE FURNISHINGS GUIDELINE		
ITEM	SUB-ITEM	DESCRIPTION
Pavements	Sidewalks	All sidewalks should be constructed with poured in place concrete high-lighted with accents or feature areas. The concrete portion of the paving would be to City and Regional sidewalk standards.
	Accent Sidewalk Paving	Accent paving would be used to highlight special areas, sidewalk edges or other special areas. Concrete unit paving of a traditional brick shape, approximately 100 mm X 200 mm should be used. Paver colours should be similar to traditional brick shades of terra cotta, brown and buff. Installation should be over a concrete slab when integral with sidewalk areas. Patterning should be simple and traditional.
	Crosswalks	Poured-in-place concrete paving or concrete unit paving with a concrete base. Concrete finishes, and pavers should be compatible with sidewalk treatments.
Lighting	Street Lights	Street lights for both Main and Ferry Streets should be the same as or similar in size and style to the Falls view district. The fixtures may be distinguished by a colour that is unique to Historic Drummondville. Traditional colours such as dark green or dark gray blue would be suitable. Light spacing along the roadway and light levels should be a function of street type and City and Regional engineering standards.
	Pedestrian Lighting	The pedestrian fixture should be unique to Historic Drummondville. The fixture should be stylistically compatible with the street light, but of the post top, not pendant style. The luminaire optics should be dark sky friendly and minimize glare at street level. The Fixture should be the same colour as the street lighting. Pedestrian scale lights should be used along both Main and Ferry streets located along the street at the mid point between street lights, on both sides of the street. This layout is intended to reinforce the pedestrian scale of the street and even out illumination between the taller street lights.
	Traffic Signal Poles	Traffic signal poles should be of the same style and colour as the family of light fixtures above, provided that all relevant engineering standards may be achieved.
Furniture	General	All furniture should be of the same stylistic family and be compatible with the heritage theme. Materials should be predominantly metal and wood. The metal components should the same colour as the family of light standards. Advertising should be discouraged on furniture items. If necessary for funding, it should be discrete and limited to one or two elements such as the waste containers or transit shelters.

STREETSCAPE FURNISHINGS GUIDELINE

ITEM	SUB-ITEM	DESCRIPTION
Furniture	Benches	Heritage style benches are 1.5 to 2 m in length. The seating surface could be either metal or wood. Sitting comfort is important which favors wood. The bench should have arm rests that will assist the disabled and elderly while deterring loitering and lying down.
	Bollards and Bicycle Lock Rings	Bollards and Bicycle lock Rings should be of a similar pole family as the light poles. Bollards should be robust and removable where emergency access is required. Bicycle Rings should be strong and located where bicycles will not impede pedestrian flow.
	Waste and Recycling Containers	Containers should be constructed of metal and conceal their contents. Multiple functions should be accommodated in a single unit.
	News Boxes	All newspaper boxes should be combined in a single container and limited to key locations where they will not impede pedestrian traffic. (i.e., away from corners). The news boxes should be constructed of metal and be colour coordinated with the lights and other metal furnishings.
Transit Shelters		Bus shelters should meet a City or Regional standard to aid in the recognition of the larger system. This furnishing item accommodates advertising most successfully, if desired to offset costs. Interior seating should be designed to discourage loitering. Although other furnishings should be historic in character, transit shelters should be understated and transparent for reasons of safety and ease of maintenance.
Landscaping	General	Streetscape trees should be selected for durability in an urban environment. Larger trees will ultimately have the most impact and will rise above shop fronts and signs. Irrigation or regular watering and feeding are required, particularly during the initial establishment years.
	Boulevard Street Trees - smaller	Bradford Pear, Little Leaf Linden and Ivory Silk Tree.
	Boulevard Street Trees – large	Honey Locust , Silver Maple and Jeffers red Maple, and Gingko.
	Display Trees for Gateways	Ornamental Pear and Plum varieties (non- fruiting) , Ivory Silk Tree, and Serviceberry.

Figure 40: Precedent - Permanent Informal Market Structures, Belleville



Figure 41: Heritage Greenway, U.K.



Figure 42: Steps to Casa Loma



3.5 PUBLIC SPACE, NODES AND LINKAGES

We recommend that Historic Drummondville be strengthened by improving the quality and acceptability of its parks and open space network. These open spaces link the neighbourhood together, and complement and contribute to a continuous public realm. The parks and open space network should be comprised of the following components:

- Public parks (i.e., Lundy's Lane Battlefield);
- Linear parks and walkways which provide a neighbourhood structure (i.e., hydro corridor, trails, pathways, and alleyway links to the market and Drummond Hill battlefield site);
- Unique streetscapes along the road rights-of-way to serve as integral pieces of the pedestrian realm (i.e., redeveloped "Main Streets"); and
- Unique focal points (i.e., the marketplace and heritage museum).

New development can contribute to, and reinforce this integrated system through contributing to the streets, courtyards, alleys, lanes, semi-private exterior spaces and rear facades facing the market, which form part of the continuous, formal and informal public parks and open space system. As development intensification occurs, opportunities to expand the existing network of open spaces and pedestrian access should be encouraged. Linkages to surrounding neighbouring precincts are important as the redevelopment of Historic Drummondville is multi-faceted and is connected to areas well beyond the immediate site. We recommend a study be undertaken to create a clear pedestrian linkage between the market and battlefield across Main Street and that the bike network be made continuous through a route along a beautified hydro corridor. As an example, Figure 43 shows public institutions, parkland, cultural features, retail destinations and desirable linkages between them.

Figure 43: Open Space Concepts and Linkages

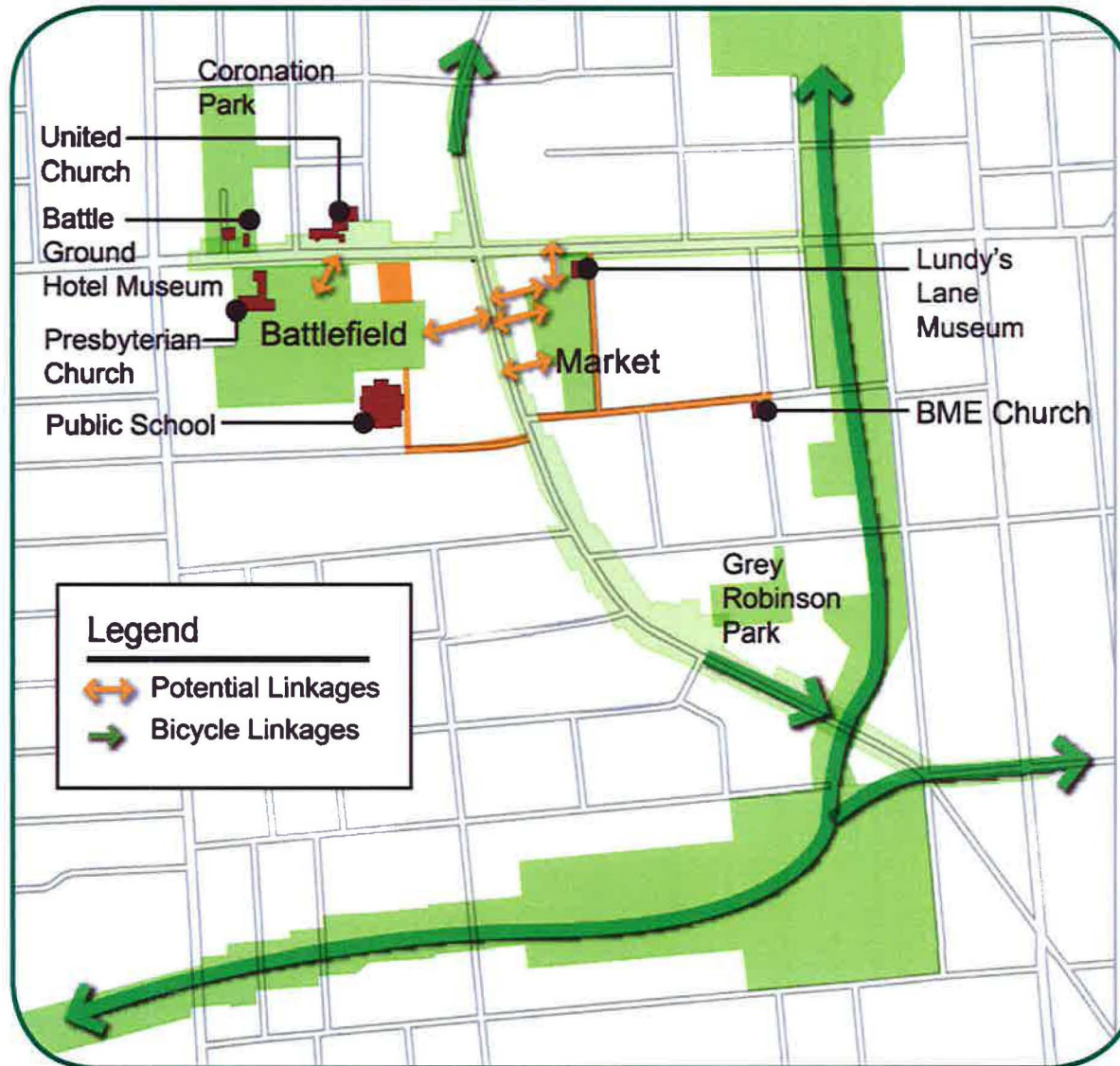


Figure 44: Current Condition - Battlefield



Figure 45: Current Condition - Link Battlefield Opportunity



Figure 46: Current Condition - Rear Facade Enhancement Opportunity

3.6 BUILT FORM

CONTEXTUAL RELATIONSHIPS

The principal objective of urban design with respect to built form is establishing the comfortable relationships between desirable existing built form and new development. This is particularly important where the prevailing community desire is to maintain the smaller scale of existing buildings. The community improvement project area is generally comprised of 2 –3 storey residential and commercial buildings. Near the intersection of Main and Ferry Streets, buildings tend to be built to the street line as shop fronts, and to the side yards as party walls. Beyond the “commercial” core, buildings tend to have front yards and be set back with front yards and side drives.

As the community improvement project area is redeveloped, new, presumably higher density building forms should be encouraged along the ‘Main Streets’. As this will occur on an incremental basis, these new building forms will need to respect the existing building form context. In this regard, the scale of new building should not overwhelm existing buildings. In general terms, this usually means that new buildings will not be more than a couple of stories taller and should take setback and architectural rhythm cues from abutting or adjacent buildings. This will imply that most new buildings will be in the low to mid-rise scale.

SCALE AND GRAIN

The texture of the city is created by both the character and form of the buildings and the ground level space between them. The immediate

neighbourhood of Historic Drummondville consists of fine grain commercial buildings punctuated by alleys and sideyards. These unique networks of public and semi-public lanes lead to mid-block spaces such as the market and surrounding parking lot. While this network of lanes and passages can attract vandalism and other criminal activity, if well designed and illuminated, criminal activity in these more intimate spaces can be virtually eliminated and these spaces can add interest and character to an area.

COMMERCIAL RETAIL FRONTAGE

Street frontage can have an enormous unifying influence on the character of the street. To reinforce and strengthen ‘Main Street’, the commercial frontages flanking it need to provide and extend a more or less continuous public-related frontage at grade with space for uses that will activate and energize the street. In essence, new projects should be designed and built to have a street presence. This goal is also appropriate for rear courtyards and passages through a site where mid block linkages are desired. Where open courtyards and public alleyways are created, retail or street frontage should wrap around the corner of these spaces.

To enhance the ‘Main Street’ characteristics of both Main Street and Ferry Streets, existing continuous street frontages should be preserved. Where development is taking place on vacant lots, continuous street frontages should be introduced. Where this is not likely to occur, the voids should be reinforced with landscaped features or other uses that will complement the public realm.

Figures 47 & 48: Current view to Main Street from within Market (above), with possible vibrant market and connection to Main Street (below)



Figures 49 & 50: Precedents - Sidewalk and Courtyard Patios

Patio uses in the Market Precinct and along Main Street edges should be encouraged in accordance with the City's cafe guidelines.



DEVELOPMENT OPPORTUNITIES

The community improvement project area possesses a number of development opportunities that could act as catalysts for the process of neighbourhood renewal. Some of these sites also hold the potential to contribute to the strengthening of urban design in Historic Drummondville. These potentials include:

1. "Main Street" Intensification – Infill and development intensification at the cross roads of Main and Ferry could bring new retail uses and a resident population to the neighbourhood centre. This location could also reinforce the notion of a market square and museum precinct.
2. Ferry Street Infill – The area between Main and Stanley streets has a number of larger, unsightly vacant lots. This area should reinforce the street linkage and complement the Victoria Avenue entertainment district.
3. Main, Stanley and Murray Infill – This area is well positioned to be the gateway from the Fallsview Tourist District to Historic Drummondville. Because it is located close to Fallsview, the scale of this block will be influenced by the larger hotels and should serve as a transition between the Fallsview District and Historic Drummondville. This area also contains the original portage route. This could spawn some very interesting architectural forms that express this diagonal trail as a unique building form and mid block linkage, either pedestrian or vehicular. The development of the block

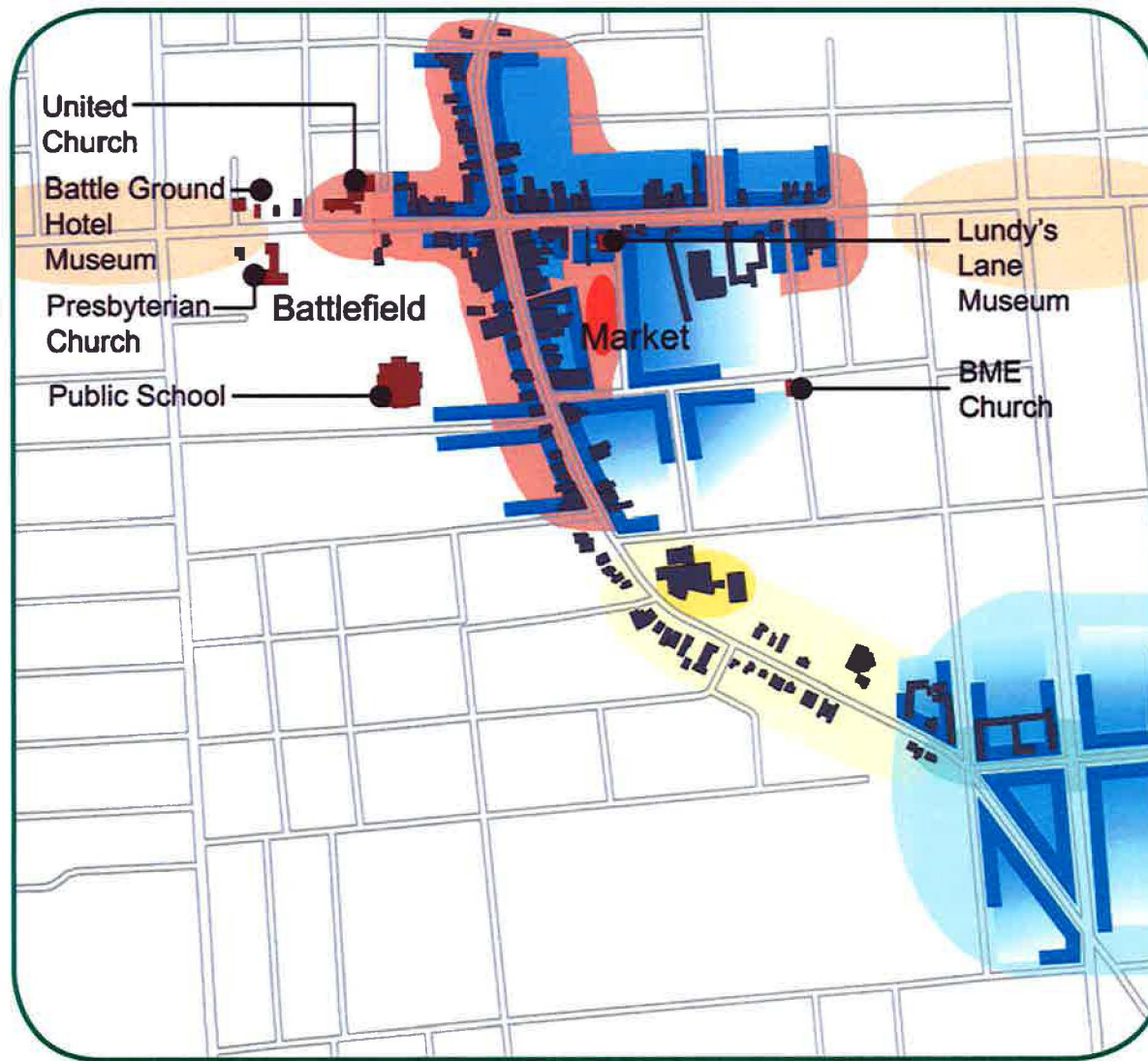
should also attempt to reconnect Stanley Avenue and Murray Hill with Main Street rather than reinforcing its disconnection.

4. The Former Coca Cola Bottling Plant – This site on Main Street sits at the transition between the 'Main Street' and 'residential village' character areas of Main Street. This site should punctuate this architectural transition while respecting the adjacent residential single family scale.

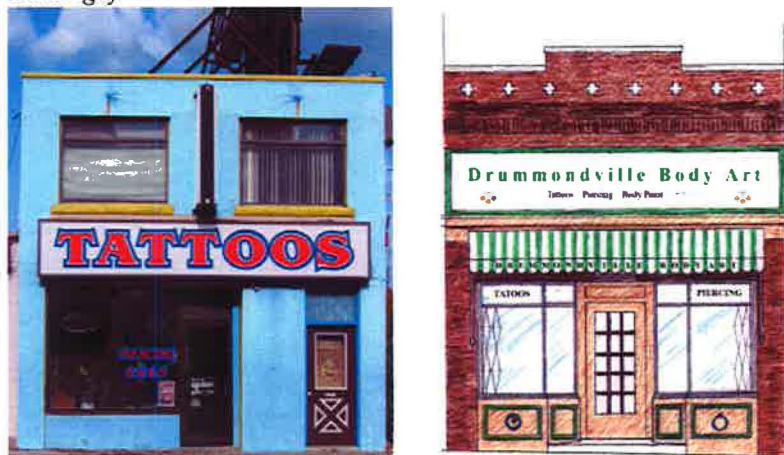
5. The National Grocers Site – While still in use, this site has significant infill development potential. To begin, the street related retail commercial development might take advantage of the main street frontage. The site is also well positioned to take advantage of the proximity to the hospital and related support services. This could be considered when formal redevelopment of this site is proposed.

While these are some of the larger potentials, the unique opportunities associated with each site or area can contribute to the success of the larger Historic Drummondville CIP.

Figure 51: Development Opportunities and Continuous Street Frontages



Figures 52 & 53: Current Condition (left) with possible Facade Improvement for Building of Same Use



With Facade Improvement and Signage all businesses, including lower order businesses can have the appearance of belonging on 'Main Street' in Historic Drummondville.

ARCHITECTURAL TREATMENTS

Facade Improvement and Signage

Currently there is little cohesion to the built form or style of the facades for the majority of buildings on Main and Ferry Streets. With a view to improving continuity and upgrading retail commercial frontages, the following are guidelines to be followed:

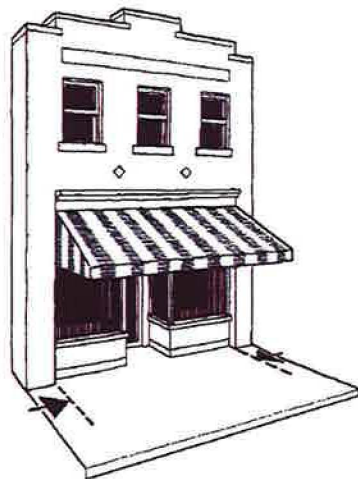
- Store frontages (doors, display windows and sign bands) should be closely joined and aligned with neighbors to create a sense of continuity;
- Shop fronts should express individuality as well as unity with the area;
- Displays and spill-out activities such as cafes should complement sidewalk activities of walking and shopping;
- There should be express mixed uses – public at grade, private uses above; and
- Store frontages and facades should be well proportioned, of human scale, oriented to the pedestrian and composed of materials of enduring quality and considered detailing.

See also: Province of Ontario, Ministry of Municipal Affairs Community Planning Wing, Design for Commercial Facade Improvements by Roger du Toit Architects and Research and Special Projects Branch.

Awnings

Together with large display windows, awnings help to diminish the barrier between inside and out by providing a physical extension of the building over the sidewalk. This creates a contained and protected public space where shoppers can step away from the passing pedestrian traffic and examine the displays or step inside a retail store. It also provides shelter for merchandise displayed on the sidewalk and outdoor seating for cafés. Consistency in storefront heights and awning locations can provide further reinforcement to the important patterns of the facades and the street wall. While awnings may not be critical to every storefront design, they can provide immediate and dramatic change at moderate costs. Depending on the design, awnings also recall an earlier time which can contribute to the streetscape theme of Historic Drummondville. Fabric awnings are superior in terms of adjustability and overall heritage character.

Figures 56: Precedent - Awnings



Figures 54 & 55: Current Architecture (top) with possible Architectural treatments to unify the Frontages (bottom)



Awnings should always be located below the storefront cornice, between and never over the storefront pilasters.